

PLAN SUMMARY

THE PURPOSE OF THIS PLAN SHALL BE TO IDENTIFY THE PROPOSED SITE DISTURBANCE, SITE GRADING, AND EROSION CONTROL MEASURES FOR THE EXPANSION OF THIS STORAGE FACILITY.

EXISTING SITE CONDITIONS

THE PROPOSED EXPANSION AREA IS CURRENTLY VACANT LAND WITH NATIVE VEGETATION. THIS PORTION OF THE OVER ALL PARCEL CURRENTLY DRAINS NORTH/NORTHWEST, TOWARD PARK AVENUE. ACCESS TO THIS SITE IS OFF OF WARNER AVENUE. ALL UTILITIES ARE READILY AVAILABLE IN THE WARNER AVENUE AND PARK AVENUE RIGHTS OF WAY, AS WELL AS WITHIN THE BOUNDARIES OF THE PARCEL.

PROJECT NOTES

1. THE APPLICANT'S CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE PROJECT SITE, INCLUDING ALL TEMPORARY SILT FENCING, DUST CONTROL, VEGETATION GROWTH ON ALL DISTURBED SURFACES, UTILITY SERVICES, ETC.
2. SITE DISTURBANCE ACTIVITIES SHALL TAKE PLACE BETWEEN AUGUST 2014 AND NOVEMBER 2014 (WEATHER PERMITTING).
3. h2 ESTIMATES THAT LESS THAN 85 CUBIC YARDS OF MATERIALS WILL BE DISTURBED DURING THE DEVELOPMENT OF THIS PLAN.
4. ACCESS TO THIS SITE SHALL OFF OF WARNER AVENUE. THIS PUBLIC RIGHT OF WAY SHALL REMAIN FREE OF CONSTRUCTION DEBRIS FROM THIS PROJECT SITE. THE APPLICANTS CONTRACTOR SHALL VISUALLY INSPECT THE RIGHT OF WAY DAILY.
5. CITY OF LEWISTON PUBLIC WORKS DEPARTMENT HAS PERMITTED THE USE OF THE ABANDONED PARK AVENUE RIGHT OF WAY FOR OVERFLOW STORMWATER COLLECTION, CONVEYANCE, AND TREATMENT. (FOR PHASE III AND PHASE IV)
6. DRY UTILITIES ARE AVAILABLE ON THE PROJECT SITE. THE CONTRACTOR SHALL EXTEND THE NECESSARY DRY UTILITIES FROM THE EXISTING ONSITE SERVICES TO THE STRUCTURES AS REQUIRED BY THE UTILITY PROVIDER.

GENERAL NOTES FOR STORMWATER SYSTEMS

- (CITY OF LEWISTON NOTES, PER DWG NO. 6-1)
1. ALL WORK SHALL CONFORM TO CURRENT IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION' (ISPCO) SPECIFICATIONS AND THE CITY OF LEWISTON STANDARDS AND SPECIFICATIONS. IN THE CASE OF CONFLICT, CITY STANDARDS SHALL PREVAIL. THE LEWISTON STORMWATER POLICY AND DESIGN MANUAL' DEFINES THE POLICIES, MINIMUM STANDARDS, REQUIREMENTS, AND PROCEDURES FOR THE DESIGN, CONSTRUCTION, AND MAINTENANCE OF PRIVATE STORMWATER SYSTEMS.
 2. ANY REPAIR, MAINTENANCE OR ALTERATION OF A PUBLIC STORM LINE OR DRAINAGE MUST BE APPROVED BY CITY ENGINEER.
 3. THE CONTRACTOR MUST SECURE APPROVAL FROM THE CITY ENGINEER PRIOR TO ADDING OR REMOVING FILL BACKFILL OVER PUBLIC STORM DRAIN MAINLINE.
 4. THE ENGINEERING INSPECTOR SHALL BE NOTIFIED AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO COMMENCING WORK ON STORM DRAINS.
 5. ALL PUBLIC STORM DRAIN LINES SHALL BE VIDEOTAPED AND SUBMITTED TO THE PUBLIC WORKS DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO PAVING. CITY SHALL HAVE FORTY-EIGHT (48) HOURS FOR REVIEW.
 6. ALL MANHOLES AND CATCH BASINS SHALL BE INSPECTED TWICE BY THE ENGINEERING INSPECTOR - ONCE PRIOR TO BACKFILL AND THEN PRIOR TO ACCEPTANCE OF IMPROVEMENTS. CALL (208) 791-8453 OR 746-1316 FOR INSPECTION SCHEDULING.
 7. CLOSED CONDUITS (OR PIPELINES) FOR STORMWATER CONVEYANCE IN THE CITY'S STORMWATER SYSTEM MUST BE A MINIMUM OF 12 INCH DIAMETER. THE PIPE MATERIAL MAY BE PLASTIC, STEEL, REINFORCED CONCRETE OR AN APPROVED MATERIAL, BUT MUST MEET CITY 605.02 SPECIFICATIONS IF PART OF THE CITY'S STORMWATER SYSTEM.
 8. MINIMUM PIPE SLOPE SHALL BE 0.5% AND MINIMUM DESIGN VELOCITY WHEN FLOWING FULL SHALL BE NOT LESS THAN 2 FEET PER SECOND AND MAXIMUM VELOCITY SHOULD NOT BE MORE THAN 8 FEET PER SECOND.
 9. JUNCTIONS OF 3 OR MORE PIPES, CHANGES IN ALIGNMENT, SLOPE AND/OR CHANGES IN PIPE DIAMETER SHALL BE MADE ONLY AT CATCH BASINS OR MANHOLES.
 10. STEEP SLOPE INSTALLATION SHALL BE INSTALLED AS PER MANUFACTURES PIPE SPECIFICATIONS.
 11. MANHOLE SHALL BE INSTALLED AT THE END OF EACH LINE, AT ALL CHANGE IN SIZE OR ALIGNMENT, AT DISTANCE NOT GREATER THAN 350 FEET OR AT CITY ENGINEER'S REQUIRED SPACING. ALL OTHER CHANGES IS SPACING MUST HAVE WRITTEN APPROVAL FROM PUBLIC WORKS DEPARTMENT.
 12. LOCATING WIRE SHALL BE INSTALLED WITH ALL PUBLIC STORM PIPE INSTALLATIONS. SEE CITY DWG 1-8, IDENTIFYING TAPE DETAIL.
 13. CATCH BASINS AND MANHOLES SHALL BE ACCESSIBLE TO VEHICLES AND EQUIPMENT FOR MAINTENANCE AND BE WITHIN RIGHT-OF-WAY OF A MINIMUM EASEMENT WIDTH OF 20 FEET. PROPOSED ACCESS ROUTE MUST BE APPROVED BY CITY ENGINEER. MAINTENANCE ACCESS ROUTES THAT DO NOT FOLLOW THE STORMLINE MUST HAVE A MINIMUM 12' EASEMENT.

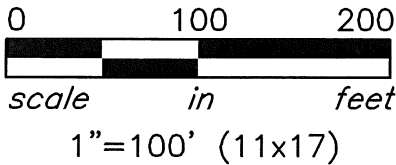
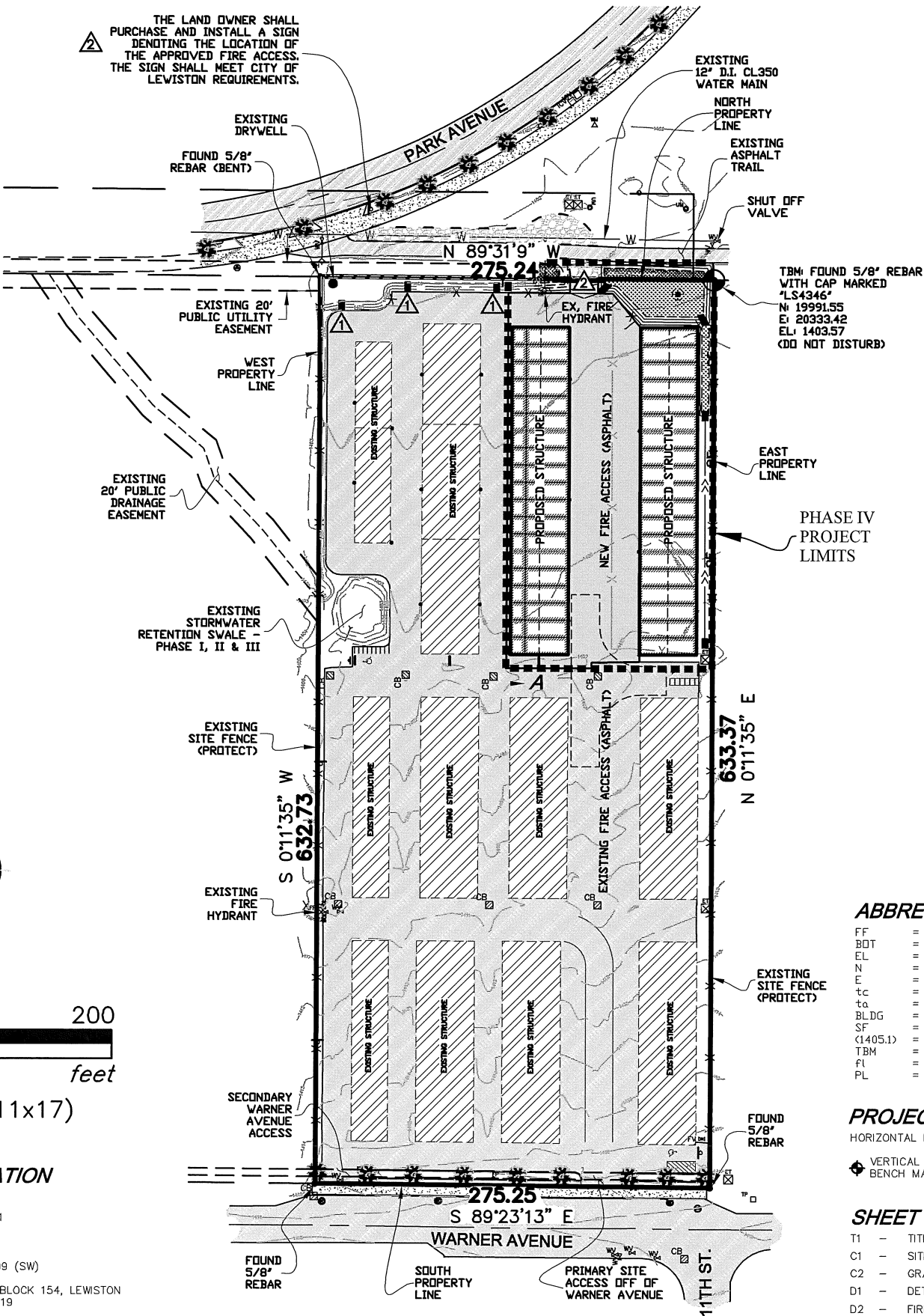
PARKING MATRIX

REQUIRED PARKING: 3 SPACES
- CH. 37 - ZONING - ARTICLE VII - OFF STREET PARKING AND LOADING:
THREE (3) SPACES PLUS ONE (1) FOR EVERY ONE HUNDRED (100)
UNITS PLUS ONE PER EMPLOYEE

PROPOSED PARKING: 3 SPACES (PHASE IV)
- SPACE DIMENSIONS: 9' x 18'

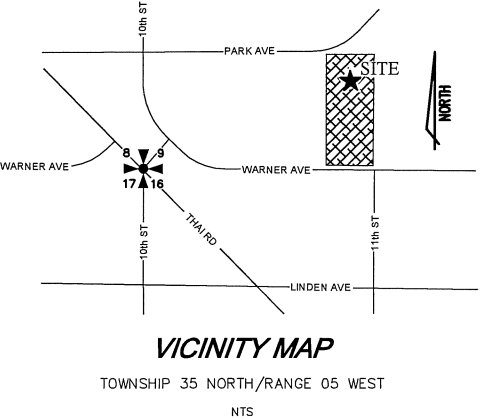
NOTE: THERE ARE EIGHT (8) EXISTING PARKING SPACES ON THIS PARCEL TO SERVE PHASE I, PHASE II, AND PHASE III OF THIS STORAGE DEVELOPMENT. WITH THE THREE (3) PROPOSED PARKING SPACES FOR THIS CURRENT PHASE (IV), THERE WILL BE A TOTAL OF ELEVEN (11) PARKING SPACES ON THIS PARCEL TO SERVE THIS DEVELOPMENT.

AIRPORT RENT A SPACE
PHASE IV - SITE DISTURBANCE PLAN



PROJECT INFORMATION

ADDRESS: 1039 WARNER AVENUE
LEWISTON, IDAHO 83501
PARCEL: RP0154008000A
TWN/RNG/SECT: 35N / 05W / 09 (SW)
LEGAL: A PORTION OF LOT 8, BLOCK 154, LEWISTON ORCHARDS TRACT NO. 19



LEGEND

- △ = Calculated point (nothing found or set)
- = Found property corner monument
- = Boundary line
- = Adjacent property line
- = Finish grade contour
- = Easement
- SF = Silt fence
- X = Perimeter fencing
- ||||| = Loading area
- 🌳 = Existing landscape tree
- ▨ = Existing asphalt
- ▩ = Proposed asphalt
- ▩ = Proposed grassy swale area
- 1405— = Existing grade contour
- ⊠ = Utility service
- ⊙ = Street/Parking lot sign
- ☼ = Existing street light
- ICV = Existing irrigation control valve
- WV = Existing water valve
- ⊙ = Existing hydrant
- CB = Existing catch basin

ABBREVIATIONS

- FF = Finish floor
- BOT = Bottom of swale
- EL = Elevation
- N = Northing coordinate
- E = Easting coordinate
- tc = Top of concrete
- ta = Top of asphalt
- BLDG = Building
- SF = Square feet
- (1405.1) = (Existing elevation)
- TBM = Temporary Bench Mark
- FL = Flow line
- PL = Property line

PROJECT DATUM NOTE

HORIZONTAL DATUM: ASSUMED
VERTICAL DATUM: NAVD88, SEE PROJECT BENCH MARK FOR FIELD CONSTRUCTION

SHEET INDEX

- T1 - TITLE SHEET & NOTE SHEET
- C1 - SITE PLAN SHEET
- C2 - GRADING PLAN SHEET
- D1 - DETAIL SHEET
- D2 - FIRE SERVICE DETAIL



revisions per City comments
dated July 2014 - sim
revisions per Fire Dept comments
dated Sept 2014 - sim

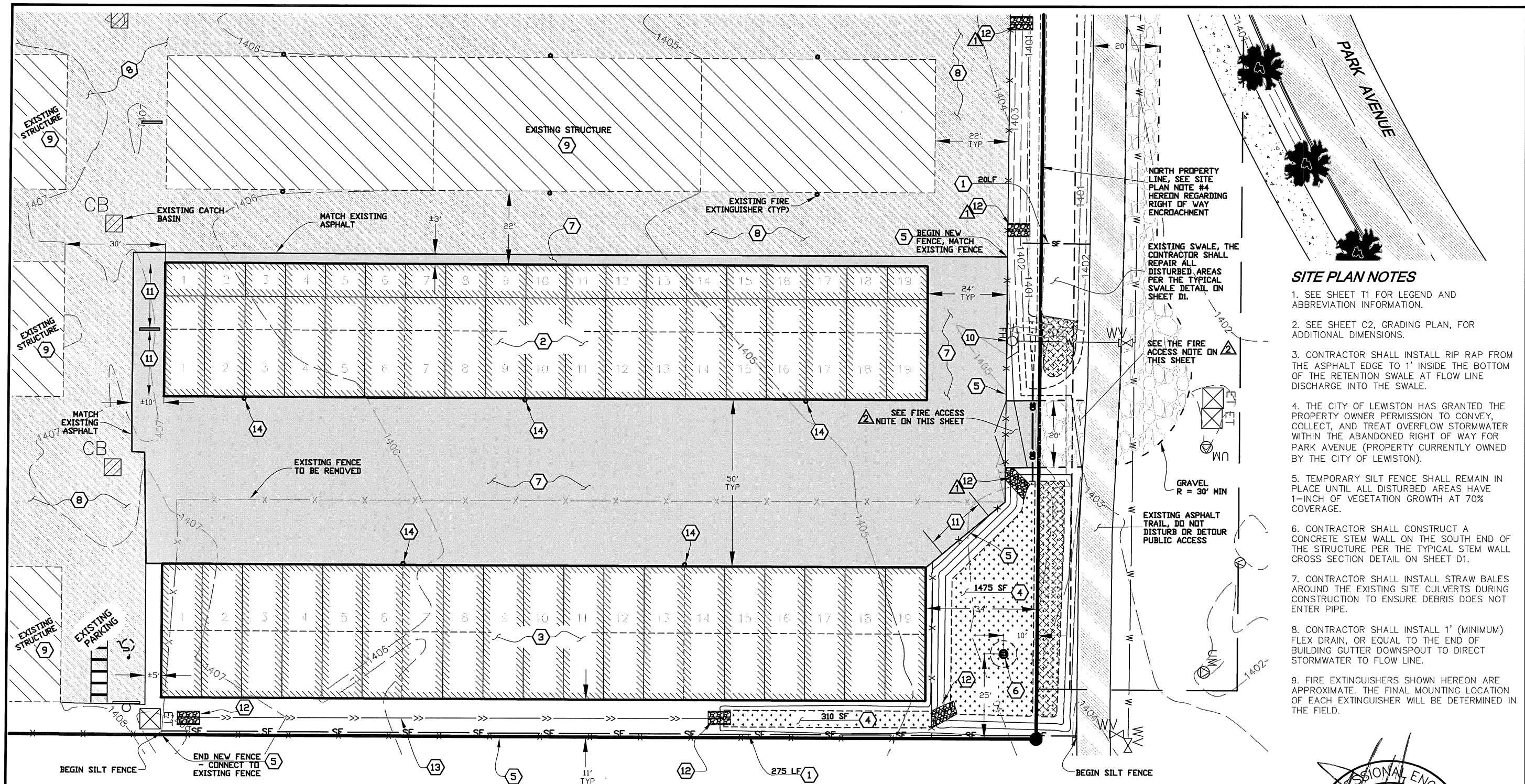


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DRAWN	blm
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AIRPORT RENT A SPACE
PHASE IV - SITE DISTURBANCE PLAN
PARCEL: RP0154008000A

TITLE & NOTES SHEET	
SHEET #	T1
	PROJECT #
2014-025	DATE
5/1/2014	



CONSTRUCTION NOTES

1. INSTALL 295LF SILT FENCING FOR EROSION CONTROL PER THE DETAIL ON SHEET D1.
2. 228' x 40' STRUCTURE PER THE APPROVED STRUCTURAL PLANS. TOTAL BUILDING SIZE SHALL BE 9,120 SF, WITH NINETEEN (19) 10'x12' UNITS, AND NINETEEN (19) 30'x12' UNITS.
3. 228' x 40' STRUCTURE PER THE APPROVED STRUCTURAL PLANS. TOTAL BUILDING SIZE SHALL BE 9,120 SF, WITH NINETEEN (19) 40'x12' UNITS.
4. CONSTRUCT A FLAT BOTTOM RETENTION AREA (SWALE) PER THE CITY OF LEWISTON DETAIL SHOWN ON SHEET D1. TOTAL SWALE ON PROPERTY: 1,785 SF (GRASS HATCHED) & TOTAL OVERFLOW IN R/W: 965 SF (CROSS-HATCHED)
5. INSTALL 330LF OF PERIMETER, SITE OBSCURING FENCING, MATCH IN KIND TO EXISTING SITE OBSCURING FENCING. ENGINEER RECOMMENDS FENCE BE INSTALLED PRIOR TO FINAL GRADING AND RETENTION AREA SEEDING.
6. INSTALL ONE (1) SINGLE DEPTH DRYWELL (SDDW / 1,000 GALLON) PER THE TYPICAL DETAIL ON SHEET D1.

7. ASPHALT PAVEMENT (EST: 14,810 SF) PER TYPICAL DETAIL ON SHEET D1. CONTRACTOR TO PROVIDE PAVEMENT SPECIFICATIONS AND WARRANTY INFORMATION TO THE LAND OWNER PRIOR TO PLACEMENT OF ASPHALT SURFACE.
8. EXISTING ASPHALT SURFACE, PROTECT AND RETAIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL REPAIR COSTS IF THE ASPHALT IS DAMAGED BY THE CONTRACTOR OR SUBCONTRACTOR DURING CONSTRUCTION.
9. EXISTING STRUCTURE, DO NOT DISTURB. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL REPAIR COSTS IF THE STRUCTURE IS DAMAGED BY THE CONTRACTOR OR SUBCONTRACTOR DURING CONSTRUCTION.
10. EXISTING FIRE HYDRANT, PROTECT AND RETAIN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL REPAIR COSTS IF THE HYDRANT OR APPURTENANCES ARE DAMAGED BY THE CONTRACTOR OR SUBCONTRACTOR DURING CONSTRUCTION.
11. 9' x 18' ASPHALT PARKING SPACE PER CITY OF LEWISTON REQUIREMENTS. THE CONTRACTOR SHALL INSTALL STRIPPING AND WHEEL STOPS PER M.U.T.C.D. REQUIREMENTS. THREE (3) TOTAL SPACES ADDED TO THIS PHASE.

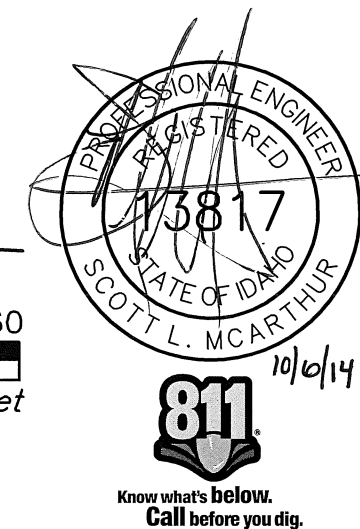
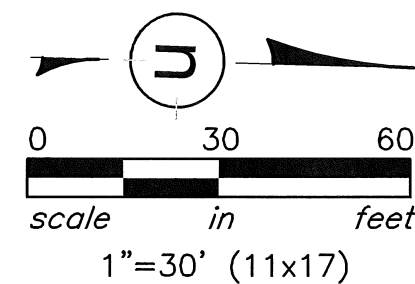
12. INSTALL 3" RIP RAP EROSION CONTROL APRON PER SITE PLAN NOTE #3 HEREON AND THE SUPPLEMENTAL TYPICAL RIP RAP INLET DETAIL PROVIDED. ALL EXISTING RIP-RAP EROSION CONTROL APRONS, THIS PHASE AND PRIOR PHASES ON THIS SITE, SHALL BE REPAIRED/REPLACED PER THIS DETAIL DURING THIS PHASE.
13. CONSTRUCT 165LF OF STORMWATER CONVEYANCE DITCH PER THE DETAIL ON SHEET D1.
14. THE CONTRACTOR SHALL PLACE CITY OF LEWISTON FIRE DEPARTMENT APPROVED FIRE EXTINGUISHERS AT APPROXIMATE LOCATIONS SHOWN. FIVE (5) NEW EXTINGUISHERS SHOWN ON THIS PLAN. THE BUILDING CONTRACTOR SHOULD CONSULT WITH THE FIRE DEPARTMENT TO CONFIRM PLACEMENT AND FINAL REQUIRED COUNT.

FIRE ACCESS NOTE

THE LAND OWNER SHALL CONSTRUCT AN ONSITE, 20' FIRE ACCESS LANE UP TO THE NORTH PROPERTY LINE. THE CITY OF LEWISTON WILL CONSTRUCT A 20' WIDE GRAVEL FIRE ACCESS LANE FROM THE PARK AVENUE RIGHT OF WAY TO THE NORTH PROPERTY LINE ADJACENT TO THE ONSITE FIRE ACCESS LANE. A 20' WIDE FIRE ACCESS GATE SHALL BE INSTALLED AS SHOWN. THE LAND OWNER SHALL PROVIDE THE FIRE DEPARTMENT WITH THE KEY OR COMBINATION TO ACCESS THE GATE.

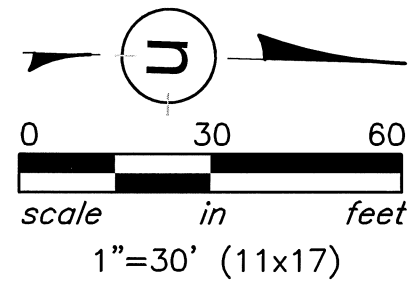
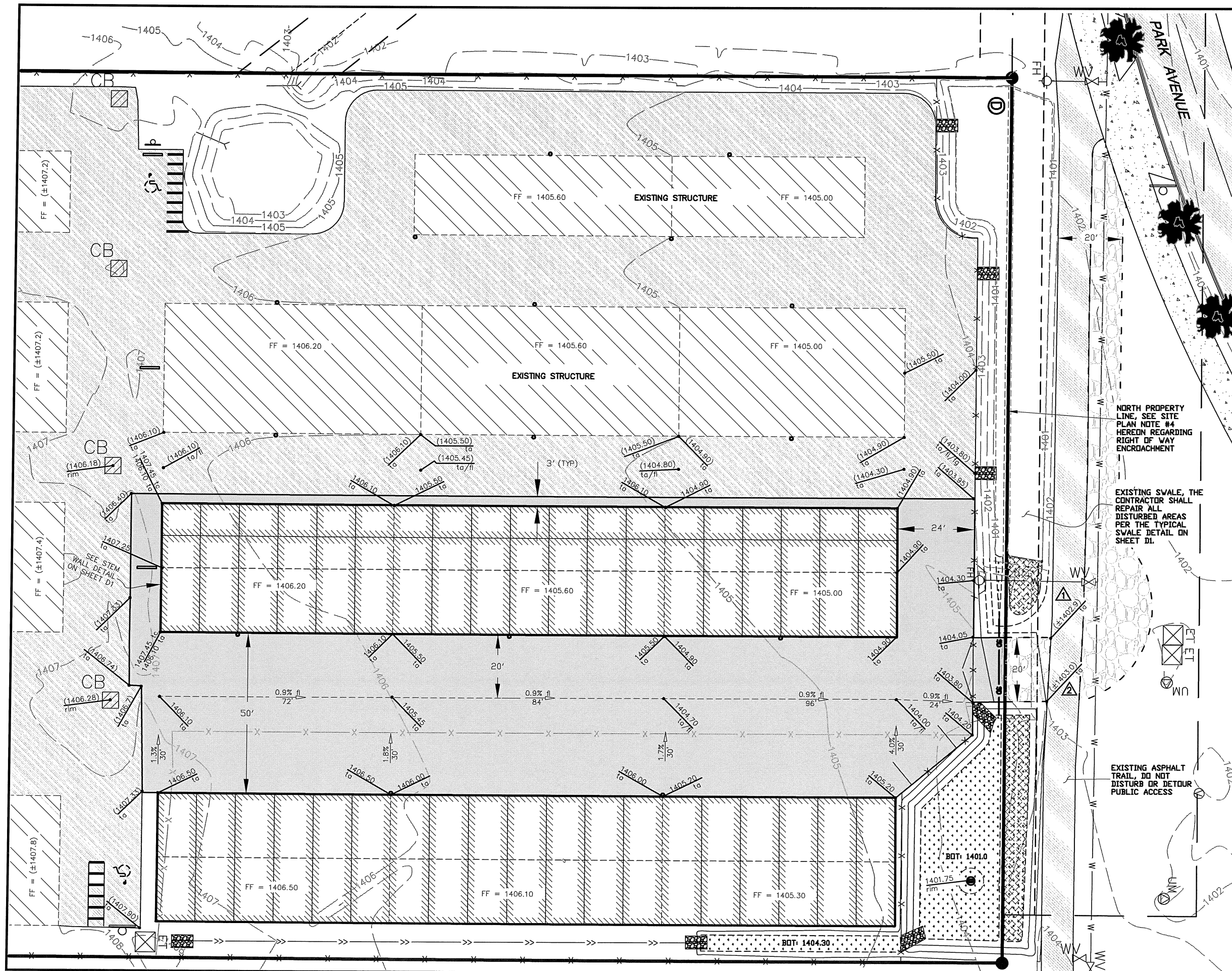
SITE PLAN NOTES

1. SEE SHEET T1 FOR LEGEND AND ABBREVIATION INFORMATION.
2. SEE SHEET C2, GRADING PLAN, FOR ADDITIONAL DIMENSIONS.
3. CONTRACTOR SHALL INSTALL RIP RAP FROM THE ASPHALT EDGE TO 1' INSIDE THE BOTTOM OF THE RETENTION SWALE AT FLOW LINE DISCHARGE INTO THE SWALE.
4. THE CITY OF LEWISTON HAS GRANTED THE PROPERTY OWNER PERMISSION TO CONVEY, COLLECT, AND TREAT OVERFLOW STORMWATER WITHIN THE ABANDONED RIGHT OF WAY FOR PARK AVENUE (PROPERTY CURRENTLY OWNED BY THE CITY OF LEWISTON).
5. TEMPORARY SILT FENCE SHALL REMAIN IN PLACE UNTIL ALL DISTURBED AREAS HAVE 1-INCH OF VEGETATION GROWTH AT 70% COVERAGE.
6. CONTRACTOR SHALL CONSTRUCT A CONCRETE STEM WALL ON THE SOUTH END OF THE STRUCTURE PER THE TYPICAL STEM WALL CROSS SECTION DETAIL ON SHEET D1.
7. CONTRACTOR SHALL INSTALL STRAW BALES AROUND THE EXISTING SITE CULVERTS DURING CONSTRUCTION TO ENSURE DEBRIS DOES NOT ENTER PIPE.
8. CONTRACTOR SHALL INSTALL 1' (MINIMUM) FLEX DRAIN, OR EQUAL TO THE END OF BUILDING GUTTER DOWNSPOUT TO DIRECT STORMWATER TO FLOW LINE.
9. FIRE EXTINGUISHERS SHOWN HEREON ARE APPROXIMATE. THE FINAL MOUNTING LOCATION OF EACH EXTINGUISHER WILL BE DETERMINED IN THE FIELD.



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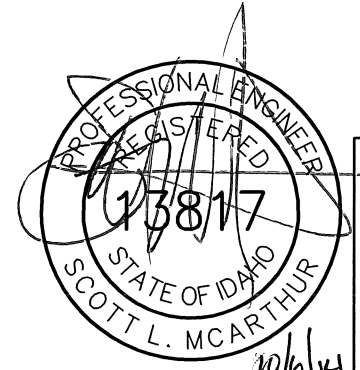
GRADING NOTES

1. ALL ELEVATIONS REFERENCE THE NAVD88 VERTICAL DATUM.
2. EXISTING CATCH BASIN RIM ELEVATIONS ARE APPROXIMATE. PROTECT ALL CATCH BASINS DURING CONSTRUCTION.
3. CHANGES TO THIS GRADING PLAN MUST BE APPROVED, IN WRITING, BY THE ENGINEER OF RECORD.
4. TEMPORARY SITE BENCH MARK (TBM) SHALL BE THE NORTHWEST PROPERTY CORNER, MONUMENTED AS A 5/8" REBAR WITH A CAP MARKED "PLS7379", WITH ELEVATION 1402.26 (NAVD88 VERTICAL DATUM)
5. SITE SLOPES ARE 2:1 UNLESS OTHERWISE NOTED ON THIS SHEET.
7. CONTRACTOR SHALL CONSTRUCT A CONCRETE STEM WALL ON THE SOUTH END OF THE STRUCTURES PER THE TYPICAL STEM WALL CROSS SECTION DETAIL ON SHEET D1.

NORTH PROPERTY LINE, SEE SITE PLAN NOTE #4 HEREON REGARDING RIGHT OF WAY ENCROACHMENT

EXISTING SWALE, THE CONTRACTOR SHALL REPAIR ALL DISTURBED AREAS PER THE TYPICAL SWALE DETAIL ON SHEET D1.

EXISTING ASPHALT TRAIL, DO NOT DISTURB OR DETOUR PUBLIC ACCESS



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**AIRPORT RENT A SPACE
PHASE IV - SITE DISTURBANCE PLAN**
PARCEL: RP0154008000A

SHEET #	C2
PROJECT #	2014-025
DATE	5/1/2014
GRADING PLAN	
CITY OF LEWISTON, IDAHO	

2", 3", 4", 5", 6", 13" GRADE RINGS
(PRODUCT #1816,17,18,19)

ADD A 6" CAST IRON RIM
(PRODUCT # 1219)

4" 2'-0" 4"

6" DIA. K.O.
12" DIA. K.O.

2'-0"

DRYWELL CONE
(PRODUCT #1801)

DRYWELL BARREL
(PRODUCT # 1802)

4'-2"

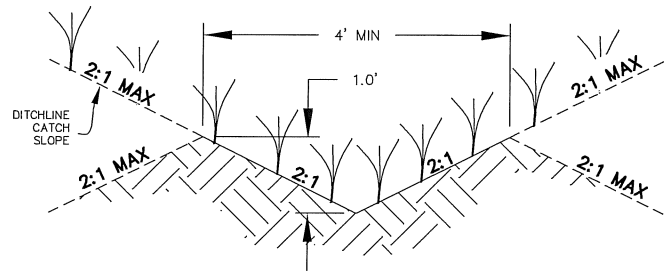
DRYWELL BARREL
(PRODUCT # 1803)

4'-4"

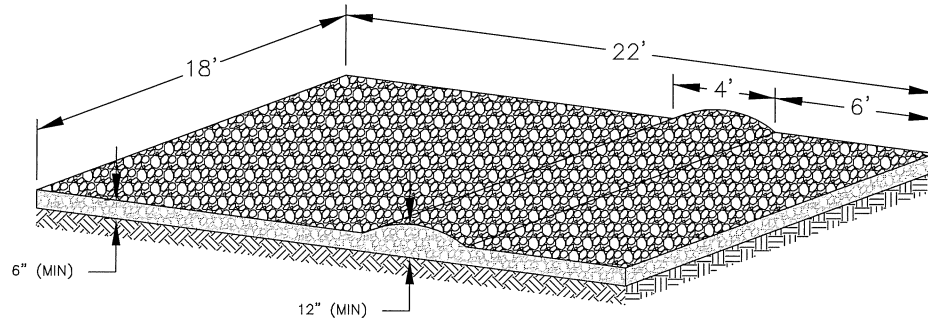
6"

4" 4'-0" 4"

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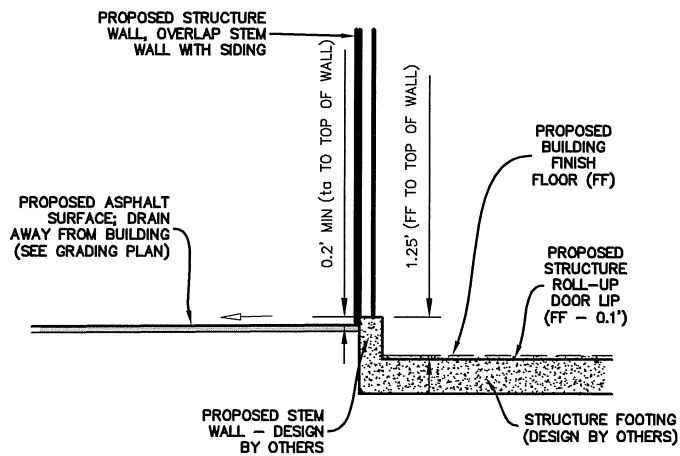


TYPICAL STORMWATER CONVEYANCE DITCH DETAIL
NTS

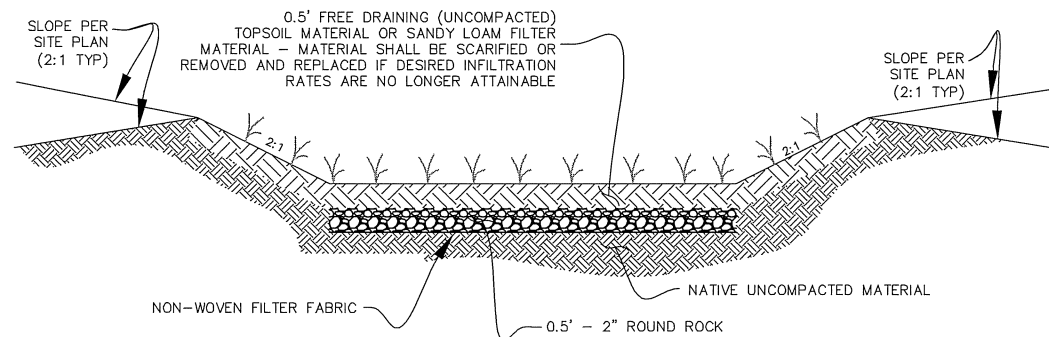


- 1) ROCK BED CONSISTS OF 6" MINIMUM DEPTH, OF 2" FRACTURED, NO FINES, RIP RAP MATERIAL.
- 2) DEBRIS BUMP CONSISTS OF 12" DEPTH OF 2" FRACTURED, NO FINES, RIP RAP MATERIAL. (4' WIDTH)
- 3) FILTER CLOTH SHALL BE PLACED ON THE NATIVE COMPACTED SUBGRADE MATERIAL PRIOR TO THE PLACEMENT OF RIP RAP MATERIAL.
- 4) TEMPORARY ROCK CONSTRUCTION ENTRANCE SHALL BE REMOVED AND DISPOSED OF PROPERLY AFTER CONSTRUCTION IS COMPLETE AND THE SITE IMPROVEMENTS ARE READY FOR FINAL ENGINEERING APPROVAL. THE DISTURBED AREA SHALL BE DRESSED WITH 6" OF 3/4" MINUS CRUSHED MATERIAL, MATCHING THE NEW ROAD IN KIND.

TEMPORARY ROCK CONSTRUCTION ENTRANCE (IF NEEDED)
NTS

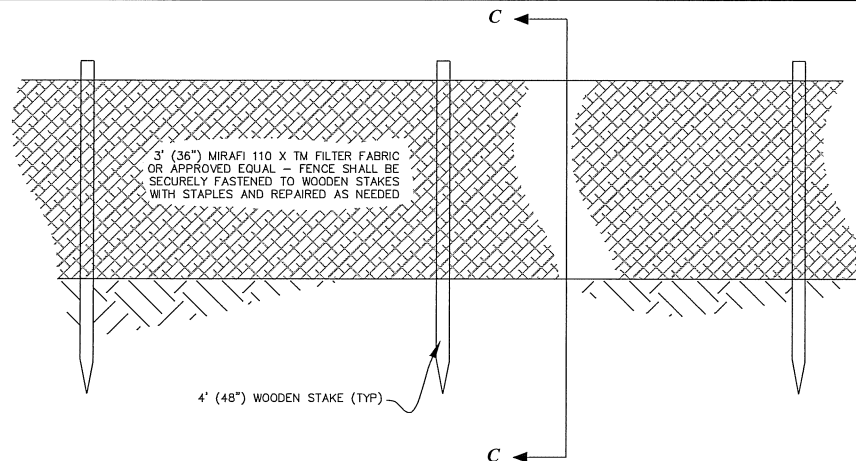


TYPICAL STEM WALL CROSS SECTION DETAIL
STRUCTURAL DESIGN BY OTHERS

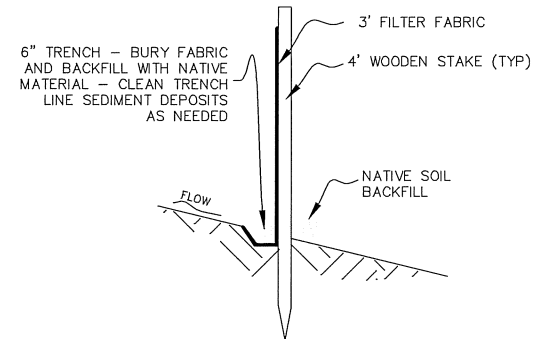


- 1) INFILTRATION RATES SHALL BE BETWEEN 0.5 IN/HR AND 3.0 IN/HR PER ENGINEER OF RECORDS RECOMMENDATIONS
- 2) DO NOT COMPACT TOPSOIL LAYER. APPLICANT TO BE HELD RESPONSIBLE FOR INSPECTING SWALE FOR PROPER INFILTRATION.
- 3) SWALE SHALL BE SEEDED WITH APPROPRIATE GRASS MIXTURE PER IDAHO BEST MANAGEMENT PRACTICES #21.
- 4) SWALE BOTTOM AREAS NOTED ON THIS PLAN REPRESENT THE FLAT BOTTOM AREA ONLY AND DO NOT INCLUDE SLOPED AREAS.

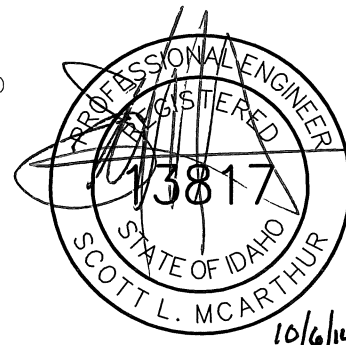
TYPICAL SWALE CROSS SECTION DETAIL
NTS



TEMPORARY SILT FENCE DETAIL
NTS



SECTION C - C



Know what's below.
Call before you dig.

revisions
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 ② revisions per Fire Dept comments dated Sept 2014 – slm

