

Date Stamped: 4/16/08

By	ALB
Revision	Comments
1	3/24/08 ADDRESS CITY'S 3/12/08 COMMENTS
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Project:  
**AARON'S SALES & LEASING**

Client:  
**JAMES ELMER CONSTRUCTION**

Project Mgr.	GC
Drawn	ZCS ALB
Drawn	RKB
Checked	AEG KB
Date	11/05/07

Sheet Contents:  
**DEMOLITION PLAN**

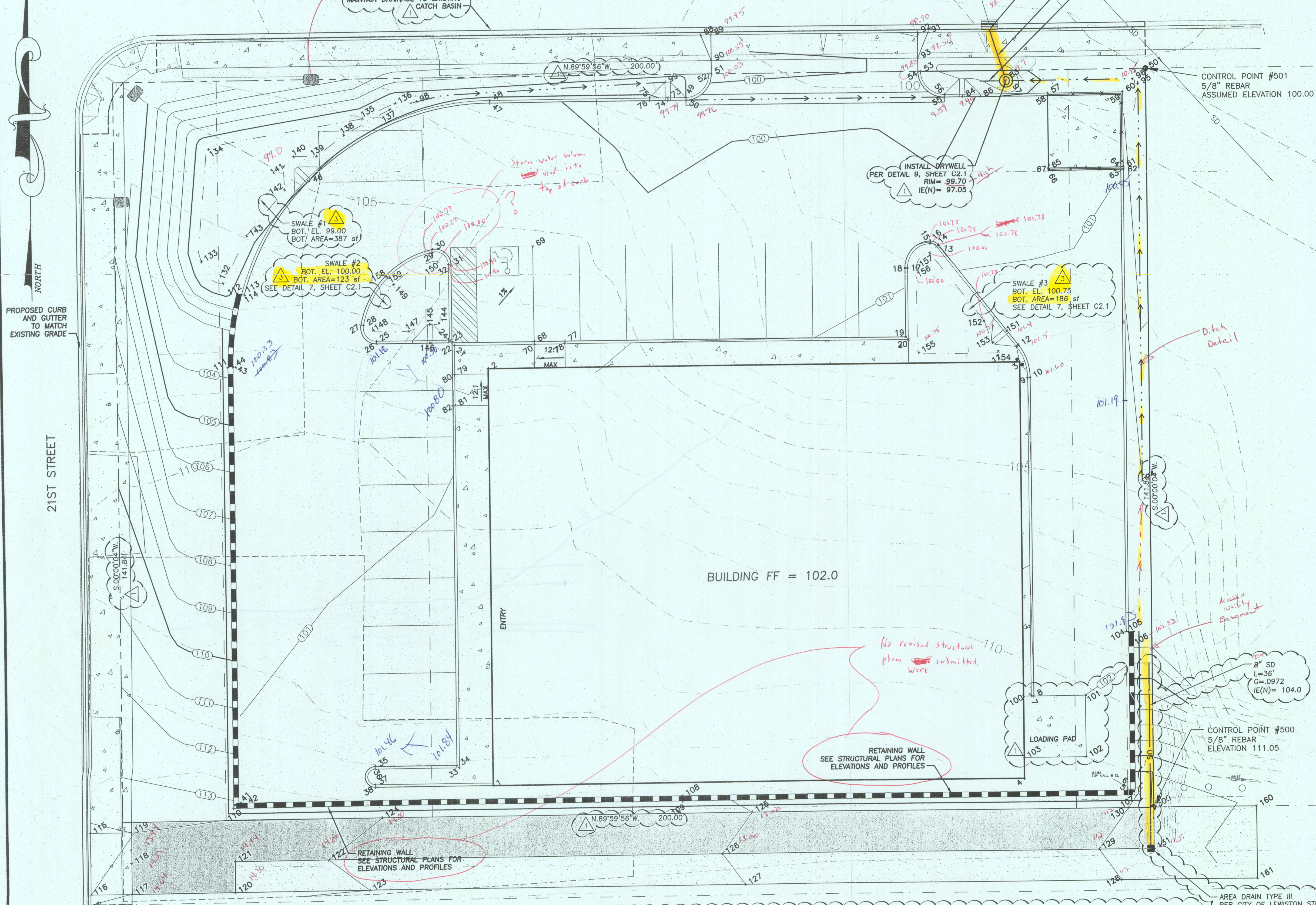
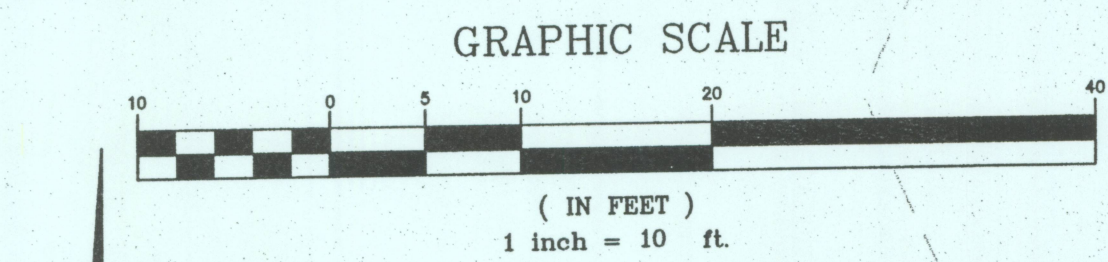
Sheet No.:

**C0.2**

USKH W.O. 1030300

#3





SWALE GRADING POINTS				
PNT#	NORTHING	EASTING	ELEVATION	DESC
132	4960.8513	4820.7967	99.00	BOS
133	4965.0494	4816.6293	99.00	BOS
134	4966.8425	4818.3628	99.00	BOS
135	4992.2803	4848.1400	99.00	BOS
136	4995.1491	4854.5754	99.00	BOS
137	4993.9951	4853.5666	99.00	BOS
138	4989.2151	4844.5075	99.00	BOS
139	4985.1673	4839.1673	99.00	BOS

SWALE GRADING POINTS (CONT.)				
PNT#	NORTHING	EASTING	ELEVATION	DESC
140	4985.1199	4834.7442	99.00	BOS
141	4983.2554	4832.8799	99.00	BOS
142	4978.6847	4832.9256	99.00	BOS
143	4969.7359	4825.4469	99.00	BOS
144	4952.7197	4852.5303	100.00	BOS
145	4952.5000	4860.7929	100.00	BOS
146	4949.0000	4860.7929	100.50	FS/INV
147	4951.2729	4856.5643	100.00	BOS
148	4951.5257	4849.7221	100.00	BOS

SWALE GRADING POINTS (CONT.)				
PNT#	NORTHING	EASTING	ELEVATION	DESC
149	4960.1369	4853.8631	100.00	BOS
150	4964.2779	4862.4743	100.00	BOS
151	4949.9722	4972.6223	101.40	FL/INV
152	4951.8543	4968.2058	100.75	BOS
153	4947.7857	4969.8892	100.75	BOS
154	4946.0334	4970.9282	100.75	BOS
155	4946.0429	4955.5429	100.75	BOS
156	4962.0000	4955.2500	100.00	BOS
157	4964.7490	4957.9194	100.00	BOS

ALLEY GRADING POINTS TABLE				
PNT#	NORTHING	EASTING	ELEVATION	DESC
115	4856.1622	4793.6752	113.3000	EXST.
116	4844.1622	4793.6752	114.0000	EXST.
117	4844.1622	4801.6752	114.6400	P
118	4850.1622	4801.6752	114.2900	P
119	4856.1622	4801.6752	113.9400	P
120	4844.1618	4821.6752	114.3600	P
121	4850.1618	4821.6752	114.1400	P
122	4850.1614	4839.7852	114.0000	P
123	4844.1603	4847.6750	114.0000	G
124	4858.1612	4850.3035	114.0000	P

ALLEY GRADING POINTS TABLE (CONT.)				
PNT#	NORTHING	EASTING	ELEVATION	DESC
125	4858.1596	4922.1234	113.0000	P
126	4850.1622	4916.3252	113.0000	P
127	4844.1622	4920.6752	113.0000	G
128	4844.1581	4993.6782	112.0000	G
129	4850.1622	4993.3252	112.0000	P
130	4858.1581	4993.3252	112.0000	P
131	4850.1580	4999.9968	111.5000	P
160	4858.1576	5019.9970	100.3000	EXST.
161	4844.1576	5019.9967	111.2000	EXST.

SURVEY CONTROL/GRADING POINTS TABLE				
PNT#	NORTHING	EASTING	ELEVATION	DESC
1	4864.0000	4872.0000	102.63	FS
2	4844.0000	4872.0000	102.00	FS
3	4844.0000	4975.0000	102.00	FS
4	4864.0000	4975.0000	102.40	FS
5	4861.5000	4995.0000	102.20	FS
6	4861.5000	4995.5000	102.70	TC
7	4875.0000	4976.5000	102.50	TC
8	4875.0000	4977.0000	102.00	FS
9	4940.9922	4976.5000	102.10	TC
10	4940.9922	4977.0000	101.60	FS
11	4946.9268	4974.4183	102.00	TC
12	4947.3391	4974.8087	101.50	FS
13	4965.9370	4959.2101	101.28	TC
14	4966.2494	4959.6005	100.78	FS
15	4966.5000	4958.0000	101.25	TC
16	4967.0000	4958.0388	100.75	FS
17	4962.0000	4953.5000	101.40	TC
18	4962.0000	4953.0000	100.90	FS
19	4949.0000	4953.0000	101.25	FS
20	4948.5000	4953.5000	101.75	TC
21	4948.5000	4865.5000	100.82	TC
22	4949.0000	4865.0000	100.82	FS
23	4949.0000	4864.6464	100.62	FS
24	4949.7071	4864.2929	101.12	TC
25	4949.5000	4850.3031	101.68	TC
26	4949.0000	4850.3031	101.18	FS
27	4952.6000	4847.3637	101.08	FS
28	4952.5000	4847.8536	101.58	TC
29	4966.1464	4861.5000	100.77	TC
30	4966.6363	4861.4000	100.27	FS
31	4963.6969	4865.0000	100.40	FS
32	4963.6969	4864.5000	100.90	TC
33	4967.5000	4865.5000	102.35	TC
34	4868.0000	4865.0000	101.85	FS
35	4868.0000	4849.0000	101.46	FS
36	4867.5000	4849.0000	101.96	TC
37	4864.5000	4849.0000	101.85	TC
38	4864.0000	4849.0000	101.35	FS
41	4861.5000	4822.5000	101.68	TC
42	4861.5000	4823.0000	101.18	FS
43	4945.0000	4823.0000	100.33	FS
44	4945.0000	4822.5000	100.83	TC
46	4980.3553	4837.6447	100.00	FS/INV
47	4995.0000	4873.0000	100.14	FS
48	4995.0000	4911.0000	100.26	TC
49	4995.0000	4911.0000	99.76	FS
51	5000.0000	4916.0000	100.03	FS
52	5000.0000	4915.5000	100.53	TC
53	5000.0000	4956.5000	100.30	TC
54	5000.0000	4956.0000	99.80	FS
55	4995.0000	4961.0000	99.59	FS
56	4995.0000	4961.0000	100.09	TC
57	4995.0000	4981.0000	100.81	TC
58	4995.0000	4981.0000	100.31	FS
59	4995.0000	4995.0000	100.45	FS
60	4995.0000	4995.0000	100.95	TC
61	4981.0000	4995.0000	100.95	TC
62	4980.5000	4995.0000	100.95	TC
63	4980.5000	4995.0000	100.45	FS
64	4981.0000	4995.0000	100.45	FS
65	4981.0000	4981.0000	100.31	FS
66	4980.5000	4981.0000	100.31	FS
67	4980.7500	4981.0000	100.81	TC
68	4949.0000	4881.0000	100.38	FS
69	4967.0000	4881.0000	100.38	FS
70	4948.5000	4881.0000	100.38	TC
73	4995.0000	4907.5000	99.79	FS/INV
74	4995.0000	4907.5000	99.79	FS/INV
75	4995.0000	4904.0000	101.33	TC
76	4995.0000	4904.0000	100.83	FS
77	4949.0000	4887.0000	100.32	FS
78	4948.5000	4887.0000	100.82	TC
79	4943.0000	4865.5000	101.21	TC
80	4943.0000	4865.0000	100.71	FS
81	4937.0000	4865.5000	101.30	TC
82	4937.0000	4865.0000	100.80	FS
83	4998.0000	4973.0000	99.70	DRYWELL
84	4995.0000	4964.5000	99.49	FS/INV
86	4995.0000	4968.0000	99.66	FS
88	5007.5653	4914.0001	100.45	TC
89	5007.5653	4916.0001	99.95	FS
90	5002.5637	4916.0000	100.55	FS
91	5007.5653	4957.9999	99.40	TC
92	5007.5653	4955.9999	99.80	FS
93	5002.5637	4956.0000	99.50	FS
94	4920.8932	4998.5000	102.00	FL
95	4997.0000	4998.5000	100.15	FL
96	4998.0000	4997.5000	100.10	FL
97	4998.0000	4974.2500	99.90	FL
98	4995.4033	4858.6865	99.98	FL
99	4998.5000	4907.0000	99.50	FL
100	4880.0000	4875.0000	102.00	CONC PAD
101	4880.0000	4987.0000	102.00	CONC PAD
102	4868.0000	4987.0000	102.10	CONC PAD
103	4868.0000	4975.0000	102.10	CONC PAD
104	4892.0000	4995.0000	101.93	FS
105	4892.0000	4995.5000	102.43	TC
106	4892.0000	4996.5000	102.93	TW
107	4860.5000	4996.5000	112.50	TW
108	4861.5000	4908.7500	101.70	FS
109	4860.5000	4908.7500	113.50	TW
110	4860.5000	4821.5000	114.50	TW
111	4945.0000	4821.5000	103.78	TW
112	4958.9441	4823.4237	101.00	TW
113	4958.6734	4824.3863	100.72	TC
114	4958.5380	4824.8677	100.22	FS
158	4961.7279	4852.2721	100.68	FS
159	4961.3744	4852.6256	101.18	TC

SURVEY CONTROL POINTS				
PNT#	NORTHING	EASTING	ELEVATION	DESC
500	4858.1581	4999.9970	111.05	CTRL PNT
501	5000.0000	5000.0000	100.00	CTRL PNT

PROFESSIONAL ENGINEER  
12577  
APR 22 2008  
CITY OF LEWISTON  
BUILDING SERVICES

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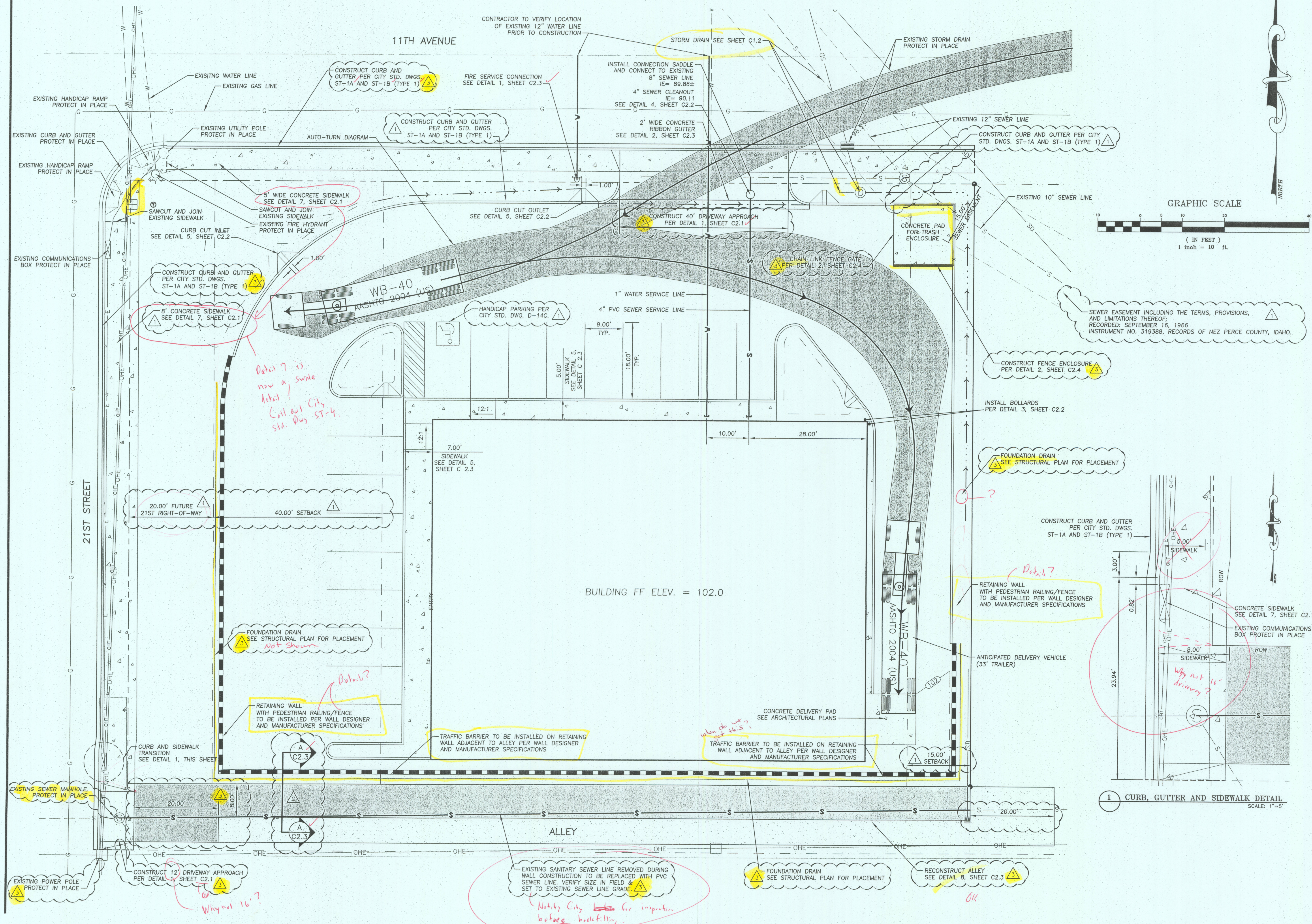
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Project Mgr.	GC
Drawn	ZCS ALB
Drawn	RKB
Checked	AEG KB
Date	11/05/07

Sheet Contents:  
**GRADING, DRAINAGE, AND SURVEY CONTROL**

Sheet No.:  
**C1.2**  
USKH W.O. 1030300





Date Stamped: 4/6/08	
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Revision	ALB
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Sheet Contents:

**SITE PLAN AND UTILITY PLAN**

Sheet No.:

C1.1

USKH W.O. 1030300

#3



SPECIFIED MATERIALS AND CONSTRUCTION PROCESSES

- 1. EMBANKMENT COMPACTION – METHOD "C" COMPACTION IS SPECIFIED
- 2. CONCRETE – CLASS 3,000 CONCRETE IS SPECIFIED
- 3. PAVEMENT – 3–1/2" PG 64–28 HOT MIX ASPHALT IS SPECIFIED
- 4. WATER PIPE: A. WATER MAIN PIPE– 12" & 10" C900 PVC  
B. SERVICE PIPE– 1.0" TYPE K COPPER TUBE PIPE
- 5. SANITARY SEWER PIPE: A. SEWER MAIN PIPE– 8" SDR 35 PVC, ASTM 3034  
B. SERVICE LATERALS– 4" SDR 35 PVC FOR SEWER SERVICE
- 6. WATER MAIN FITTINGS – SHALL BE CAST IRON OR DUCTILE IRON CONFORMING TO AWWA STANDARDS C153. ALL FITTINGS SHALL BE RATED FOR A MINIMUM WORKING PRESSURE OF 150 PSI.
- 7. GATE VALVES – SHALL CONFORM TO AWWA C509 RESILIENT WEDGE GATE VALVES
- 8. BEDDING – PIPE SHALL BE BEDDED WITH IMPORTED CRUSHED AGGREGATE
- 9. CONTRACTOR TO SUBMIT ASPHALT MIX DESIGN TO THE CITY FOR APPROVAL

TESTING & INSPECTION

- 1. ALL TESTING IS THE RESPONSIBILITY OF THE CONTRACTOR, RESULTS SHALL BE PROVIDED TO THE CITY & THE ENGINEER.

GENERAL STREET CONSTRUCTION NOTES

- 1. STREET BASE ROCK LIFTS SHALL BE IMPORTED CRUSHED ROCK, COMPACTED TO A 95% RELATIVE DENSITY.
- 2. STREET BASE ROCK LIFTS SHALL BE NO MORE THAN 8" IN HEIGHT IN ORDER TO ASSURE PROPER COMPACTION. APPROPRIATE MOISTURE SHALL BE ADDED TO THE ROCK FOR OPTIMIZING COMPACTION. ROCK IS NOT TO BE COMPACTED OR GRADED DRY.
- 3. A COAT OF CSS–1 TACK SHALL BE APPLIED TO THE ADJOINING EDGES OF ALL ASPHALT STREET CUTS.
- 4. ASPHALT COURSE SHALL BE NO LESS THAN 3" THICK OR AS SHOWN ON PLAN DETAILS, PLACED IN TWO SEPARATE 1½" COMPACTED LIFTS AND SHALL SLOPE FROM CENTERLINE TO THE EDGE OF THE STREET AT A 2% SLOPE OR AS NOTED.
- 5. CURB RAMP TRANSITIONS SECTIONS SHALL BE NO STEEPER THAN A 12:1 SLOPE AS PER THE AMERICANS WITH DISABILITIES ACT (A.D.A.)
- 6. CURB RAMP SHALL BE PLACED TO FACILITATE ALIGNMENT OF THE CROSSWALKS. CURB RAMPS SHALL NOT BE PLACED INTEGRAL WITH THE SIDEWALK OR CURB AND SHALL BE ISOLATED WITH EXPANSION JOINT MATERIAL.
- 7. DRIVEWAY/ALLEY APPROACH LIP SHALL BE NO LESS THAN 1%. WHEELCHAIR RAMP LIP SHALL BE NO MORE THAN 1/4".
- 8. ALL RETROFIT CURB, GUTTER & SIDEWALK WORK SHALL BE SAW CUT SMOOTHLY AND EVENLY AT THE ADJOINING EDGES. COMPLETELY REMOVE THE WHOLE CURB AND GUTTER. CURB, GUTTER, DRIVEWAY & GUTTER SHALL NOT BE POURED AS ONE SECTION.
- 9. FELT EXPANSION MATERIAL SHALL BE PLACED AT JOINTS PRIOR TO AND AFTER THERE IS ANY CHANGE IN DIRECTION, PROFILE, OR OTHER SIGNIFICANT CHANGE IN THE DESIGN OR CONFIGURATION OF THE SIDEWALK AND/OR CURB OCCURS. EXAMPLES OF THIS ARE AS FOLLOWS:  
DRIVEWAY/ALLEY APPROACHES  
OBSTRUCTIONS SUCH AS MANHOLES, WATER VALVE BOXES, POWER POLES, PHONE PEDESTALS, BEFORE AND AFTER ANY CHANGE IN DIRECTION SUCH AS SWEEPS OR CORNERS.  
WHEEL CHAIR RAMPS
- 10. CURB, GUTTER AND SIDEWALKS SHALL BE POURED AS SEPARATE UNITS, MONOLITHICALLY CONSTRUCTED WORK SHALL NOT BE ALLOWED.

GENERAL WATER UTILITY NOTES

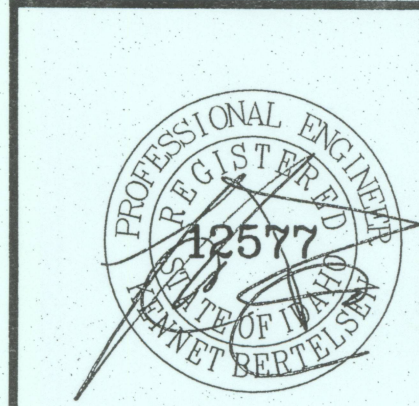
- 1. PIPE: ALL PIPE FOR WATER MAINS SHALL BE C900 PVC. PIPE 8" AND LARGER SHALL BE CLASS 150.
- 2. IDENTIFYING TAPE: IDENTIFYING TAPE SHALL BE USED WITH ALL WATER MAINS AS PER STANDARD PLAN.
- 3. RESTRAINED JOINTS: THRUST BLOCKS SHALL BE USED UNLESS RESTRAINED JOINTS ARE SPECIFICALLY AUTHORIZED BY WATER DISTRIBUTION SUPERVISOR OR CITY ENGINEER. RESTRAINED JOINTS SHALL BE "MEGALUG" OR "FIELD–LOK". A MINIMUM OF THREE JOINTS SHALL BE RESTRAINED FROM EACH FITTING, EACH WAY.
- 4. WATER SYSTEM SHUTDOWNS: THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO THE WATER DIVISION 5 FULL WORKING DAYS IN ADVANCE OF A REQUESTED SHUTDOWN FOR RESIDENTIAL SERVICES. SEVEN FULL WORKING DAYS OF ADVANCE NOTICE ARE REQUIRED WHERE COMMERCIAL SERVICES ARE AFFECTED. A WORKING DAY SHALL BEGIN AT 8AM. A MINIMUM OF 24 HOURS IN ADVANCE OF THE SHUT DOWN (72 HOURS FOR RESTAURANTS, HOTEL, ETC.), THE CONTRACTOR SHALL DISTRIBUTE NOTICES (SUPPLIED BY WATER DIVISION) TO THE EFFECTED WATER USERS. THE WATER DIVISION WILL DETERMINE THE REQUIRED SHUTDOWN AREA AND SHALL TURN ALL MAIN DISTRIBUTION VALVES. SERVICE VALVES SHALL BE TURNED BY THE CONTRACTOR.
- 5. POLYETHYLENE SLEEVING: IN AREAS OF CLAY SOILS, OR CORROSIVE SOILS, THE WATER MAIN SHALL BE PROTECTED BY POLYETHYLENE SLEEVING IN ACCORDANCE WITH ANSI/AWWA C105/A21.5.
- 6. WATER MAIN TAPPING SLEEVES: WHEN A TAP MUST BE PERFORMED ON A WATER MAIN 3" IN DIAMETER AND LARGER, MUST USE A ROMAC SST STAINLESS STEEL TAPPING SLEEVE WITH DUCTILE IRON FLANGE.
- 7. WATER VALVES: RESILIENT SEAT GATE VALVES TO BE INSTALLED ON ALL BRANCHES AND AT ALL INTERSECTIONS FOR 4"–10" DIAMETER PIPING. BUTTERFLY VALVES TO BE USED FOR 12" AND LARGER DIAMETER VALVES. THE AFOREMENTIONED SHALL BE STANDARD UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
- 8. ALL PIPE AND SERVICES SHALL BE INSTALLED WITH CONTINUOUS TRACER TAPE INSTALLED 12" TO 18" UNDER THE FINAL GROUND SURFACE.
- 9. ALL WATER MAINS SHALL BE STAKED FOR GRADES AND ALIGNMENT BY AN ENGINEERING OR SURVEYING FIRM CAPABLE OF PERFORMING SUCH WORK.
- 10. WHERE CONNECTIONS REQUIRE "FIELD VERIFICATION", CONNECTION POINTS WILL BE EXPOSED BY CONTRACTOR AND FITTINGS VERIFIED 48 HOURS PRIOR TO DISTRIBUTING SHUTDOWN NOTICES.
- 11. AT ANY CONNECTION TO AN EXISTING LINE WHERE A NEW VALVE IS NOT INSTALLED, THE EXISTING VALVE MUST BE PRESSURE TESTED PRIOR TO CONNECTION. IF AN EXISTING VALVE FAILS TO PASS THE TEST, THE CONTRACTOR SHALL MAKE THE NECESSARY PROVISIONS TO TEST THE NEW LINE PRIOR TO CONNECTION TO THE EXISTING SYSTEM OR INSTALL A NEW VALVE.
- 12. EXISTING PIPELINE LOCATIONS AND CONDITIONS SHOWN ON THE PLANS AND DETAILS ARE BASED ON ASSUMED CONDITIONS. PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL FIELD VERIFY ACTUAL LOCATIONS, SIZES, AND CONDITIONS OF EXISTING PIPING, VALVES, AND FITTINGS AND SUPPLY PROPER MATERIALS TO CONSTRUCT COMPLETE AND OPERABLE CONNECTIONS. MATERIALS THAT DIFFER FROM THOSE SHOWN ON DETAILS SHALL BE DEEMED INCIDENTAL.
- 13. HORIZONTAL AND VERTICAL PIPING ALIGNMENT SHOWN ON PLANS IS APPROXIMATE. IT SHALL BE THE CONTRACTOR'S OPTION TO RELOCATE SAID PIPING AS REQUIRED TO AVOID GEOGRAPHIC OBSTACLES. ANY SUCH REALIGNMENT SHALL BE APPROVED BY THE ENGINEER, BE KEPT WITHIN THE ROAD RIGHT–OF–WAY OR EASEMENT, SHALL CONFORM TO PIPE DEPTH REQUIREMENTS PRESCRIBED IN THE SPECIFICATIONS, AND SHALL BE PERFORMED AT NO ADDITIONAL EXPENSE TO THE OWNER.
- 14. ALL ELBOWS REQUIRED FOR VERTICAL ELEVATION CHANGE NOT SPECIFICALLY SHOWN ON THE PLANS SHALL BE DEEMED INCIDENTAL TO THE PROECT AND ALL COSTS INCLUDED IN THE APPROPRIATE BID ITEMS. SAID ELBOWS SHALL BE RESTRAINED MECHANICAL JOINT FITTINGS OR APPROVED EQUAL.
- 15. ALL WATER MAINS AND SERVICE LINES SHALL BE INSTALLED AT A MINIMUM DEPTH OF 30"
- 16. ALL WATER PIPE SHALL HAVE 4" OF CRUSHED ROCK FOR BEDDING. BACKFILL OVER THE PIPE MAY BE SUBSTITUTED WITH CLEAN, ROCK FREE DIRT AND SHALL BE SUBJECT TO COMPACTION REQUIREMENTS AND APPROVAL BY THE CITY OF LEWISTON.

FIRE DEPARTMENT NOTES

- 1. THE FOLLOWING NOTES REFER TO REQUIREMENTS OF THE LEWISTON FIRE DEPARTMENT. QUESTIONS CONCERNING THESE NOTES SHOULD BE DIRECTED TO LINDA STEPUTAT, (208) 743–3554.
- 2. PROJECT SHALL COMPLY WITH THE PROVISIONS OF IFC 105.3.6 – COMPLIANCE WITH CODE.
- 3. SIZE AND TYPE OF FIRE SERVICE MAINS SHALL BE APPROVED BY THE FIRE DEPARTMENT PRIOR TO INSTALLATION.
- 4. ALL FIRE DEPARTMENT INSPECTIONS ARE TO BE REQUESTED THROUGH THE PERMIT CENTER, PLEASE BE SPECIFIC AS TO TYPE OF INSPECTION REQUESTED. ALL SITE INSPECTIONS REQUIRE A MINIMUM 24 HOURS NOTICE.
- 5. FIRE DEPARTMENT CONNECTION (FDC) THREADS SHALL BE PROTECTED WITH APPROVED KNOX FDC PLUGS. BEGIN APPLICATION PROCESS WITH THE FIRE DEPARTMENT PRIOR TO PERMIT ISSUANCE.

SANITARY & STORM SEWER NOTES

- 1. STATIONS SHOWN ON PLANS ARE REFERENCED TO MANHOLE CENTERLINES. PIPELINE DIRECTIONAL NOTATION (I.E. N.S.E.W.) AT MANHOLE ARE SHOWN FOR ORIENTATION PURPOSE ONLY.
- 2. SEWER LINE PAY LIMIT SHALL BE MEASURED HORIZONTALLY FROM CENTER TO CENTER OF MANHOLE.
- 3. PLACE 20 SQUARE FEET OF VISQUEEN BEFORE POURING BASE WHEN GROUNDWATER EXISTS.
- 4. PRE CAST BASES SHALL BE USED WHENEVER POSSIBLE. IF NECESSARY TO CAST IN PLACE AND WITH ENGINEERS APPROVAL, USE CLASS 4000 CONCRETE.
- 5. LOWER PRE CAST CONCRETE RING INTO BASE AND LEVEL BEFORE CONCRETE IS SET.
- 6. ALLOW A MINIMUM OF 24 HOURS TO ELAPSE BEFORE PLACING REMAINING RINGS AND CONE.
- 7. JOINT SEAL MATERIAL SHALL BE TYLOX 'SUPER SEAL' OR APPROVED EQUAL. JOINTS SHALL BE FURTHER SEALED WITH 12" WIDE 'BESTSEAL WRAP' JOINT SEALANT FROM BESTFITT BASKET CO.
- 8. WHERE CONCRETE OR DUCTILE IRON PIPE IS USED, STANDARD COUPLINGS SHALL BE PROVIDED FOR FLEXIBLE CONNECTIONS TO MANHOLES.
- 9. ALL 'U' SHAPED CHANNEL SHALL BE CONSTRUCTED IN THE MANHOLE BASE BY USE OF A PROPERLY SHAPED FORM.
- 10. BRANCH LINE INVERTS SHALL NORMALLY BE D/2 ABOVE THE INVERTS OF THE MAIN CHANNEL AT THE JUNCTION UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- 11. MANHOLES SHALL HAVE STEPS OF ½ INCH DIAMETER DEFORMED BARS WHICH SHALL BE EMBEDDED IN A RESILIENT, CORROSION RESISTANT RUBBER WHICH MEETS OR EXCEEDS ALL REQUIREMENTS OF ASTM C478 AND OSHA. THEY SHALL BE WEDGLOK TYPE WL–11 OR APPROVED EQUAL.
- 12. CONSTRUCT MANHOLE LID 18" ABOVE EXISTING GROUND IN AREAS OUTSIDE R.O.W. WHEN SHOWN ON PLANS OR REQUIRED BY ENGINEER.
- 13. NO PICK HOLES IN PREFORMED MANHOLES. USE PICK BALLS THAT ARE FORMED INTO THE BARRELS.
- 14. FOR A 48" DIAM. MANHOLE THE MAXIMUM PIPE SIZE ALLOWABLE IS 21" PIPE DIAMETERS LARGER THAN 21" MUST BE APPROVED BY THE CITY ENGINEER.
- 15. MANHOLE PIPE CONNECTORS SHALL BE CAPABLE OF A 10 INCH DEFLECTION IN ANY ONE DIRECTION AND SHALL BE INSTALLED AS REQUIRED BY THE MANUFACTURER. THE A–LOK PIPE CONNECTOR, MANUFACTURED BY A–LOK PRODUCTS, AND PSX, MANUFACTURED BY PRESS–SEAL GASKETS CORP. ARE PRE–APPROVED. ALL OTHER CONNECTION SYSTEMS MUST BE APPROVED BY THE CITY ENGINEER PRIOR TO USE.
- 16. ANY GAPS, HOLES, ROUGH SPOTS, ETC., IN THE CHANNELS, AT PIPE CONNECTIONS, AND JOINTS, SHALL BE FILLED OR REPAIRED IN THE FIELD.
- 17. MANHOLES SHALL BE SET A MINIMUM OF 6 INCHES AND NOT MORE THAN 12" BELOW FINISH GRADE AND THEN ADJUSTED TO GRADE WITH RISER RINGS AS REQUIRED.
- 18. IN MANUFACTURING THE MANHOLES, THE CONTRACTOR IS ADVISED TO REVIEW THE DETAILS AS OUTLINED IN ANY TECHNICAL SPECIFICATIONS AND/OR PLANS, WHICH SHOW THE SEWER PIPE SLOPE CALCULATED TO THE CENTERLINE OF THE MANHOLE.
- 19. MASTIC JOINT SEAL SHALL BE PLACED AT EVERY JOINT BETWEEN BARREL SECTIONS, RISER SECTIONS, AND THE CAST IRON TOP.
- 20. PRE–CAST MANHOLE BASE SHALL BE NO LESS THAN 24" IN HEIGHT. A THICKNESS OF 6" SHALL BE MAINTAINED BETWEEN THE FLOW CHANNEL AND BASE ROCK. BASE SECTIONS SHALL BE PRE–CAST, CUSTOM UNITS FITTED WITH "A–LOK", PSX GASKETS OR A "KOR–N–SEAL" BOOT IN PLACE AROUND ALL ATTACHED PIPE SECTIONS. WHERE A "KNOCK OUT" VS. A "PRE–CAST" HOLE MUST BE PLACED, A SAND COLLAR OR AN APPROVED EQUAL SHALL BE USED FOR PIPE CONNECTION. THE SAND COLLAR SHALL BE PROPERLY SEALED INSIDE AND OUTSIDE WITH NON–SHRINK, QUICK SET GROUT.
- 21. SHOULD THE ENGINEER DETERMINE THE NATIVE MATERIAL IS UNSUITABLE FOR FOUNDATION, SPECIFIED MATERIAL (FOUNDATION MATERIAL CLASS B) SHALL BE IMPORTED AND PLACED AS OUTLINED IN WSDOT TECHNICAL SPECIFICATIONS 9–03.17 OR BY THE CITY ENGINEER.
- 22. CURB INLET BOXES SHALL BE NO LESS THAN 48" IN HEIGHT. A THICKNESS OF 6" SHALL BE MAINTAINED BETWEEN THE INSIDE BASE AND BASE ROCK. CURB INLET BOXES SHALL BE PRE–CAST CUSTOM UNITS CONSTRUCTED WITH A REINFORCEMENT CAGE AND PUNCH OUT SECTIONS NO LESS THAN 2" THICK ON ALL FOUR VERTICAL SIDES.
- 23. THE CITY ENGINEER MAY REQUIRE THE APPLICATION OF AN AIR PRESSURE TEST ON ANY SEWER PIPES. CLEANING AND REMOVAL OF ANY ROCK IS REQUIRED. AFTER CLEANING, THE CITY SHALL BE NOTIFIED FOR TV INSPECTION OF ALL SEWER LINES. ANY WORK FAILING AN AIR TEST AND OR TV INSPECTION MUST BE REPAIRED AND RE–TESTED FOR CITY APPROVAL. THE CITY ENGINEER OR DESIGNEE MUST WITNESS, VERIFY, AND RECORD ALL TESTS.



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Revision	
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Comments	

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Project:  
**AARON'S SALES & LEASING**

Client: JAMES ELMER CONSTRUCTION		
Project Mgr.	GC	
Drawn	ZCS	ALB
Drawn	RKB	
Checked	AEG	KB
Date	11/05/07	

Sheet Contents:  
**CONSTRUCTION NOTES (CONT.)**



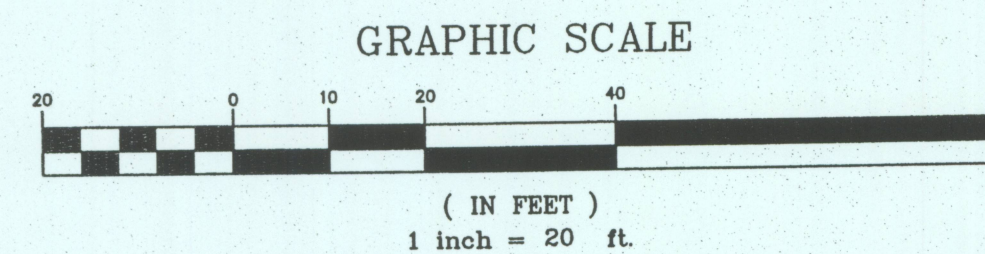
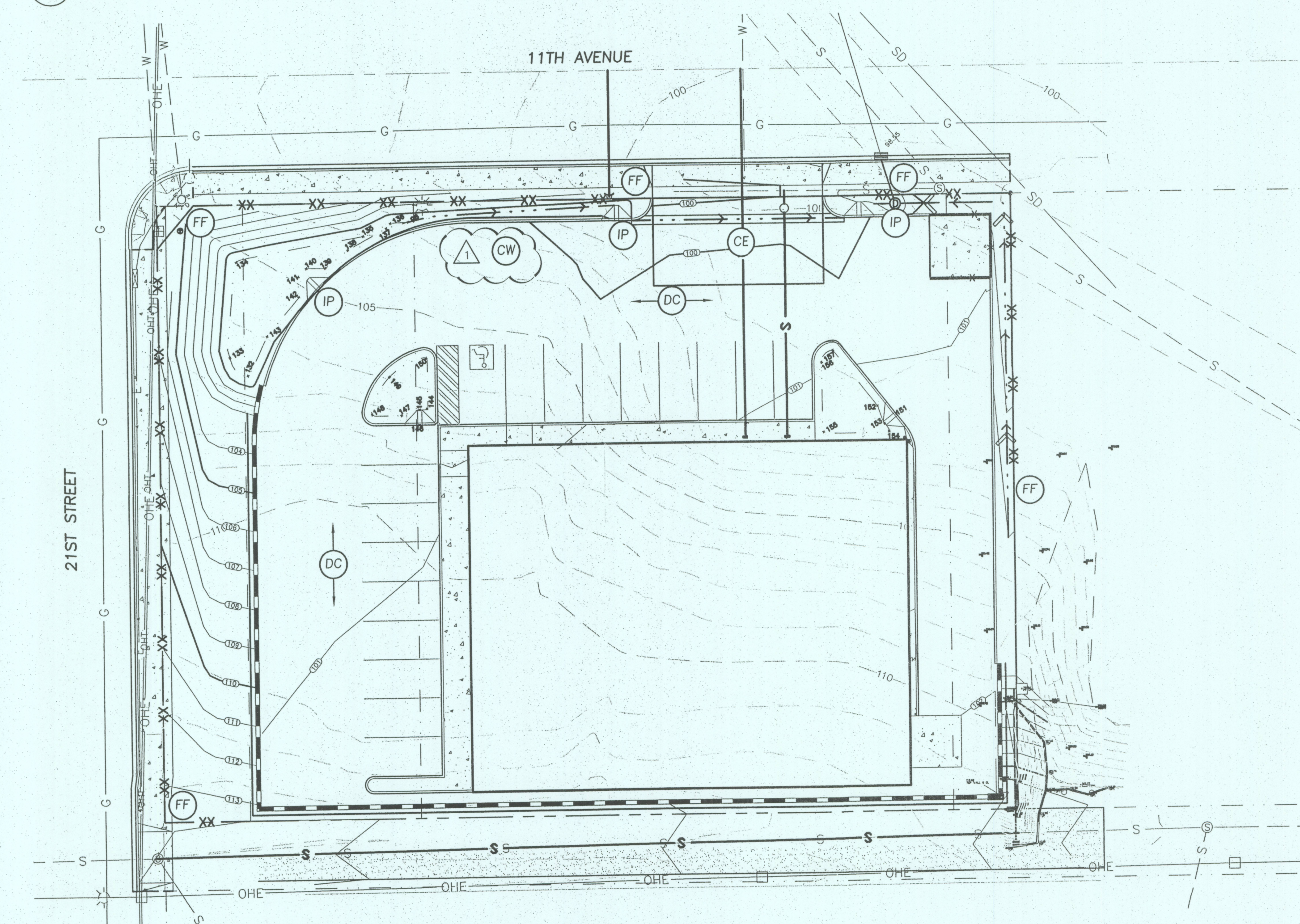
CODE	SYMBOL	DESCRIPTION
SO		SODDING, BMP-21
TO		TOP SOILING, BMP-20
CE		CONSTRUCTION ENTRANCE BMP-5, SEE DETAIL 2, THIS SHEET
DC		DUST CONTROL, BMP-7
FF		FILTER FENCE, BMP-36
STB		STRAW BALE BARRIER, SEE DETAIL 4, THIS SHEET
IP		INLET PROTECTION SEE DETAIL 3, THIS SHEET
CW		CONCRETE WASHOUT AREA, BMP-13

# 1 EROSION CONTROL LEGEND

# 2 CONSTRUCTION ENTRANCE

# 3 INLET PROTECTION AND FILTER FENCE

# 4 TYPICAL STRAW BALE BARRIER



## STORM WATER POLLUTION PREVENTION PLAN

DURING CONSTRUCTION, THE CONTRACTOR SHALL USE BEST MANAGEMENT PRACTICES CONFORMING TO IDAHO CATALOG OF STORM WATER BEST MANAGEMENT PRACTICES FOR IDAHO CITIES AND COUNTIES TO REDUCE OR ELIMINATE BLOWING DUST, EXCESSIVE RUNOFF, AND SOIL EROSION ACROSS PROPERTY LINES AND INTO STREETS AND RIGHT OF WAY, AND TO ELIMINATE TRACKING SOIL AND MUD ONTO STREETS FROM CONSTRUCTION EQUIPMENT AND VEHICLES.

THE DEVELOPER IS RESPONSIBLE FOR STREET CLEANUP AT THE END OF EACH SHIFT.

THESE MEASURES MAY INCLUDE BUT ARE NOT LIMITED TO THE FOLLOWING.

BMP- 1 TIMING OF CONSTRUCTION: THE CONTRACTOR SHALL LIMIT WORK DURING AND FOLLOWING STORM EVENTS TO PREVENT MOBILIZATION OF SEDIMENTS OFF THIS SITE.

BMP- 5 STABILIZATION OF CONSTRUCTION ENTRANCE/EXIT: THE CONTRACTOR SHALL USE EXISTING PAVED ACCESS ONTO CITY STREETS. TRACKING OF SEDIMENT ONTO THE STREET WILL BE CLEANED UP BY THE CONTRACTOR. WASHING OF TRACKED SEDIMENT INTO THE STORM DRAIN IS PROHIBITED. STORM DRAIN IS IDENTIFIED BY ALL CONVEYANCE OF STORM WATER INCLUDING STREETS, CURB AND GUTTER, OPEN DRAWS, ETC.

BMP- 7 DUST CONTROL: THE CONTRACTOR SHALL PREVENT THE GENERATION OF DUST TO THE MAXIMUM EXTENT PRACTICABLE. ANY VISIBLE DUST LEAVING THE CONSTRUCTION PROPERTY WILL TRIGGER THE IMPLEMENTATION OF BMPs TO CONTROL DUST, TO INCLUDE BUT NOT LIMITED TO, APPLICATION OF WATER, COVERING OPEN SOIL AREAS, AND LIMITING EARTH DISTURBING ACTIVITIES DURING WINDY TIMES.

BMP- 10 SPILL PREVENTION AND CONTROL: THE CONTRACTOR SHALL MINIMIZE THE EXPOSURE OF POLLUTANTS TO STORM WATER RUNOFF BY ENCLOSING ANY DRIPS, OVERFLOWS, LEAKS, AND OTHER LIQUID MATERIAL RELEASES OR BY ISOLATING POLLUTANT SPILLS FROM STORM WATER RUNOFF.

BMP- 13 CONCRETE WASTE MANAGEMENT: THE CONTRACTOR SHALL MINIMIZE STORM WATER POLLUTION FROM CONCRETE WASTE BY CONSTRUCTING A WASHOUT AREA AWAY FROM STORM DRAINS,, DITCHES OR WATER BODIES.

BMP-36 SILT FENCE. THE CONTRACTOR SHALL USE SILT FENCE AS A TEMPORARY BARRIER TO ASSIST IN THE INTERCEPTION OF SEDIMENT WHILE ALLOWING WATER TO PERCOLATE THROUGH.

BMP-41 EARTH DIKE: EARTH DIKES SHALL BE USED TO CHANNEL STORM WATER TO A DESIRED LOCATION. ANY EARTHEN DIKES INSTALLED TO CHANNELIZE WATER SHOULD BE IDENTIFIED IN THE SWPPP AT THE TIME OF THEIR CONSTRUCTION.

BMP-42 PERIMETER DIKE/SWALE: TEMPORARY PERIMETER DIKES/SWALES SHALL BE USED TO PREVENT POLLUTANT LADEN STORM WATER FROM LEAVING THE SITE.

SHOULD SPECIFIC STORM EVENTS CAUSE MOBILIZATION OF SEDIMENT, THE CONTRACTOR SHALL FURNISH OTHER BMP's AS REQUIRED.

### STANDARD NOTES

1. APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G. SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
2. THE IMPLEMENTATION OF THESE EROSION/SEDIMENTATION CONTROL PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE EROSION/SEDIMENTATION CONTROL FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/LANDSCAPING IS ESTABLISHED.
3. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE APPLICANT/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
4. THE EROSION/SEDIMENTATION CONTROL FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
5. THE EROSION/SEDIMENTATION CONTROL FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE EROSION/SEDIMENTATION CONTROL FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DO NOT LEAVE THE SITE.
6. THE EROSION/SEDIMENTATION CONTROL FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING. WRITTEN, SIGNED, AND DATED INSPECTION REPORTS SHALL BE KEPT WITH THE SWPPP.
7. THE EROSION/SEDIMENTATION CONTROL FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN THE 48 HOURS FOLLOWING A MAJOR STORM EVENT. ANY INACTIVE AREAS SHALL BE STABILIZED.
8. AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
9. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

Date Stamped: 4/16/08

By	ALB	ALB
Revision	Comments	Comments
1	12/6/08	ADDRESS CITY'S 1/30/08
2	3/24/08	ADDRESS CITY'S 3/12/08

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Client:  
**JAMES ELMER CONSTRUCTION**

Project Mgr.	GC
Drawn	ZCS ALB
Drawn	RKB
Checked	AEG KB
Date	11/05/07

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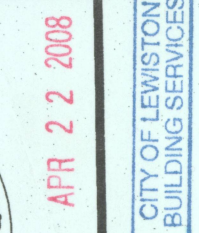
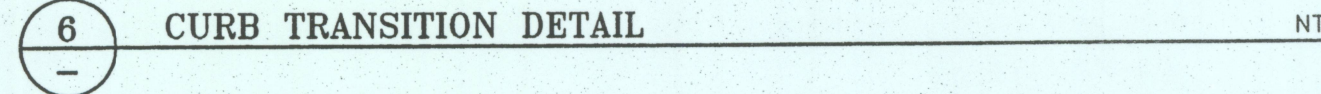
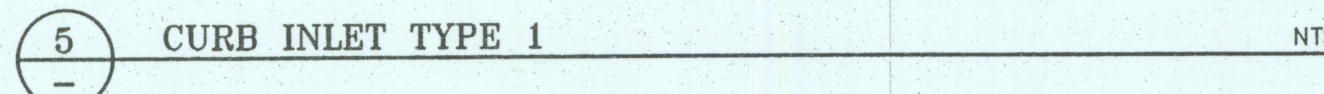
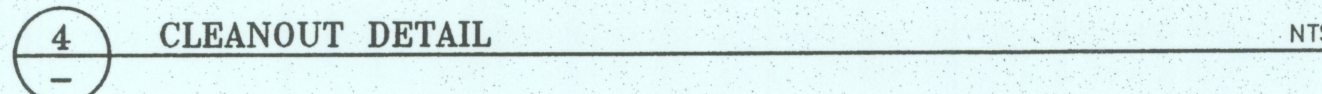
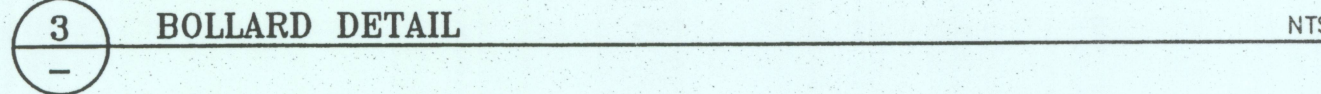
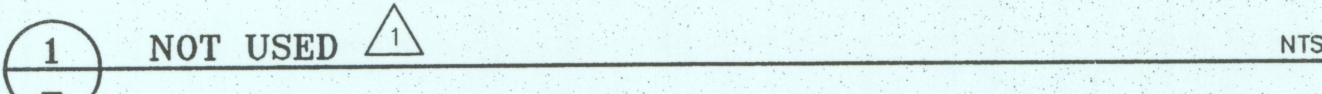
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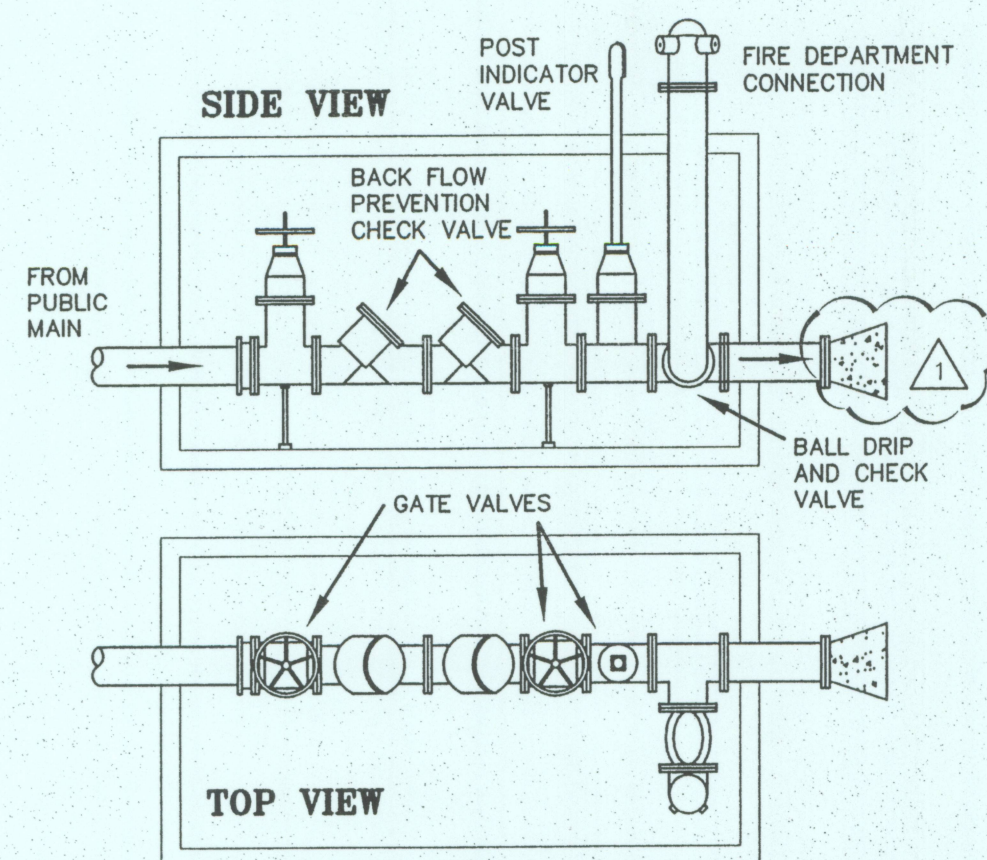
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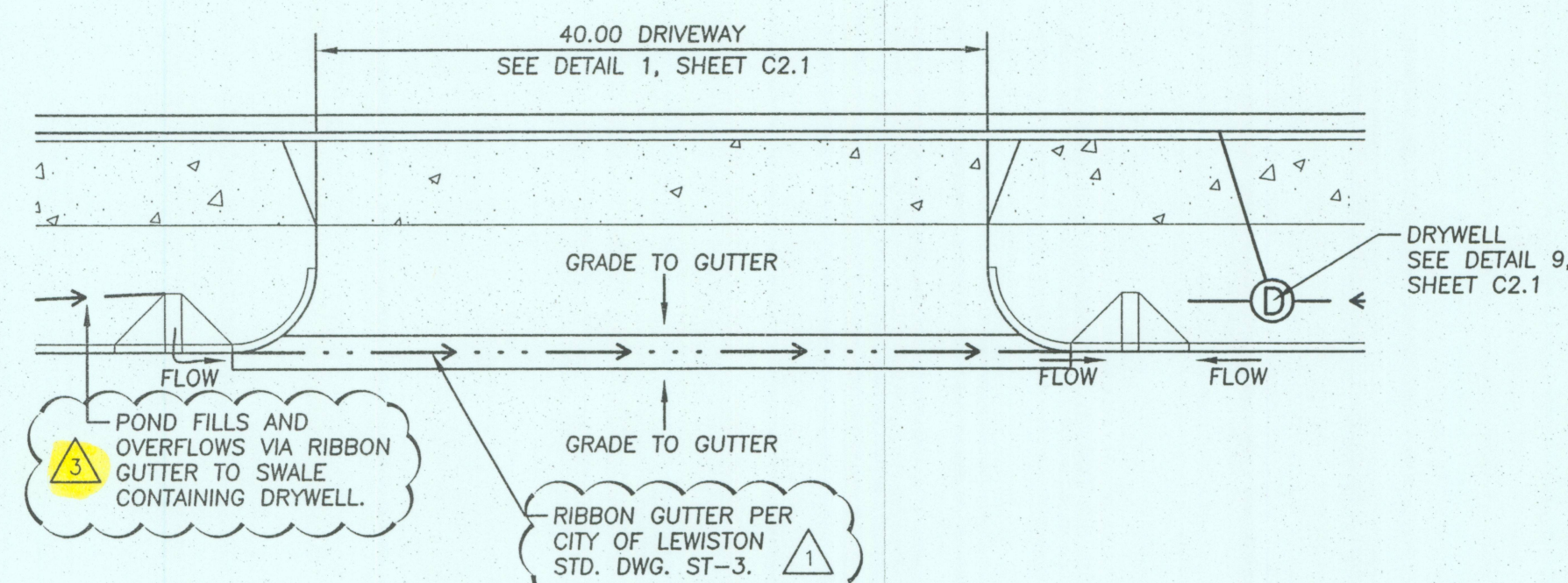




1. SEE NFPA 24 FOR PRIVATE FIRE SERVICE MAINS AND THEIR APPURTENANCES.
2. TAMPER SWITCHES SHALL BE REQUIRED ON ALL CONTROL VALVES (NFPA 13).
3. POST INDICATOR VALVE SHALL BE PROVIDED.

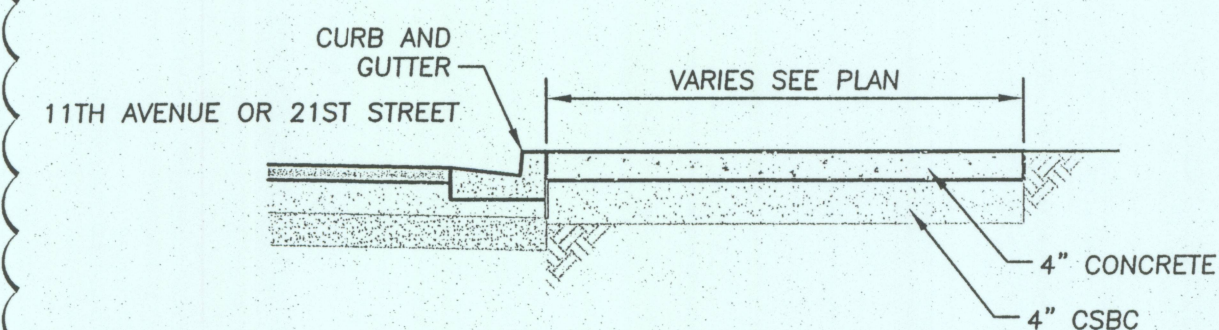
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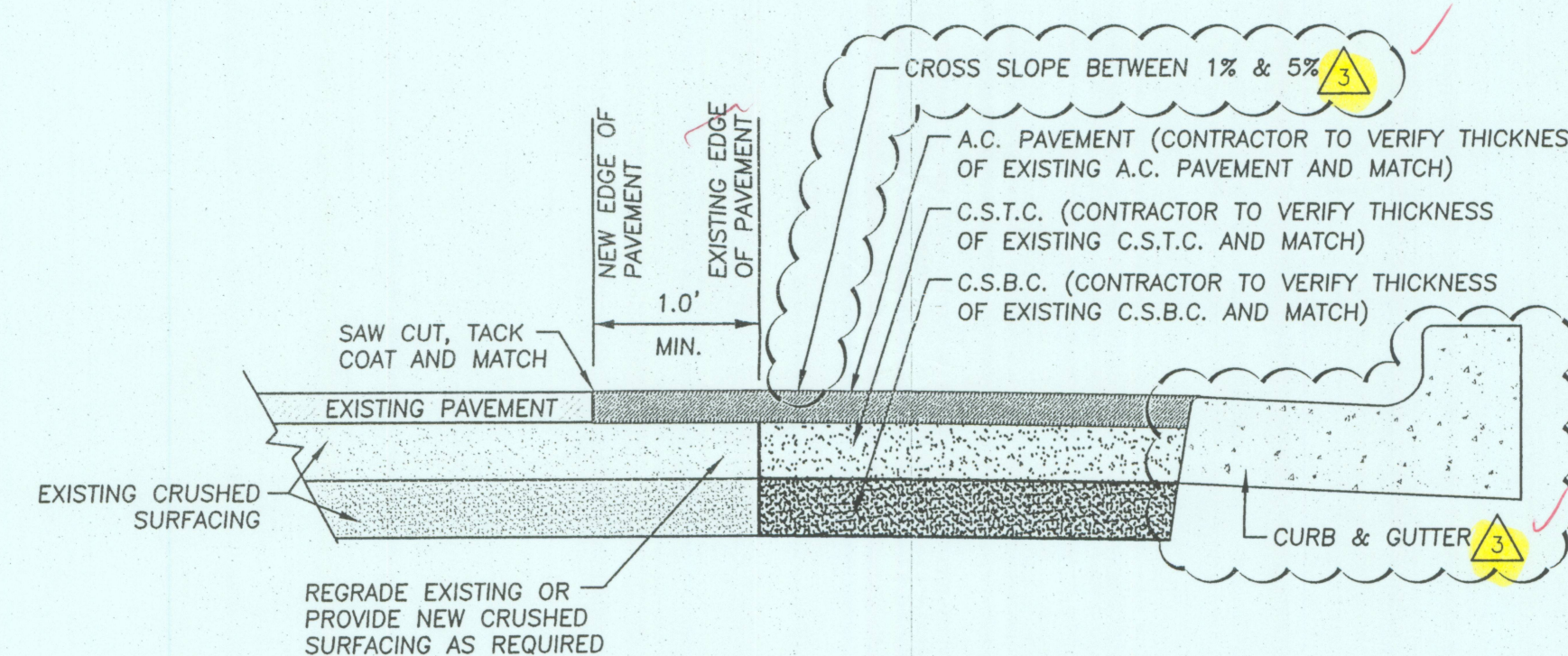
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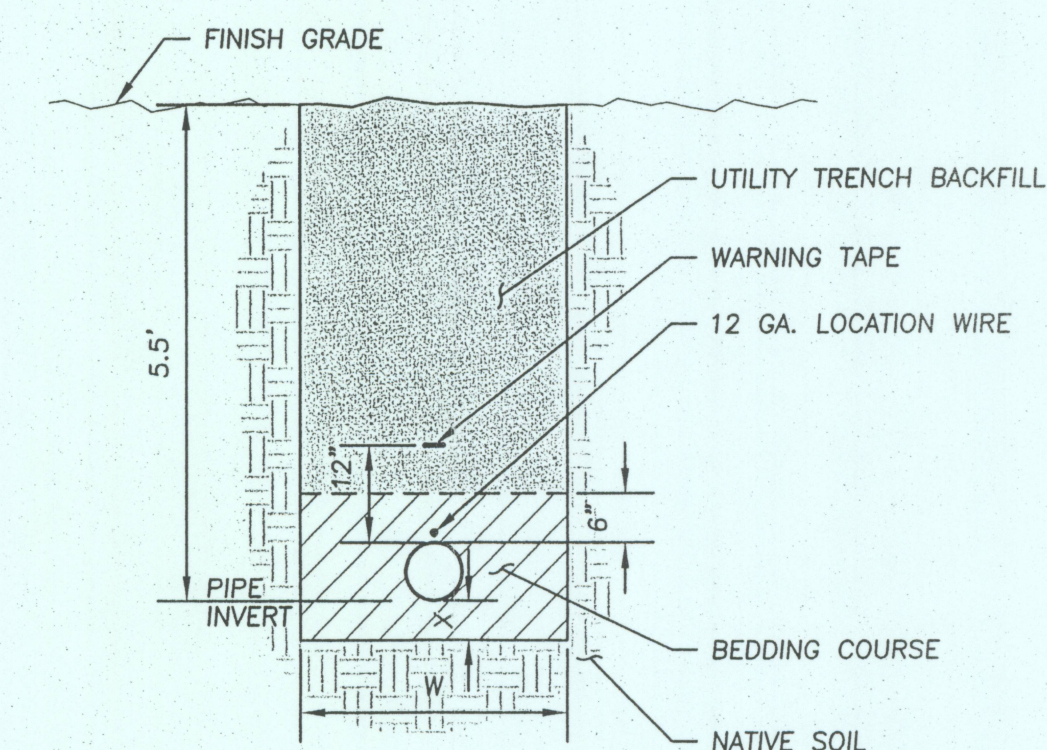
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4 PAVEMENT CUT AND MATCH DETAIL

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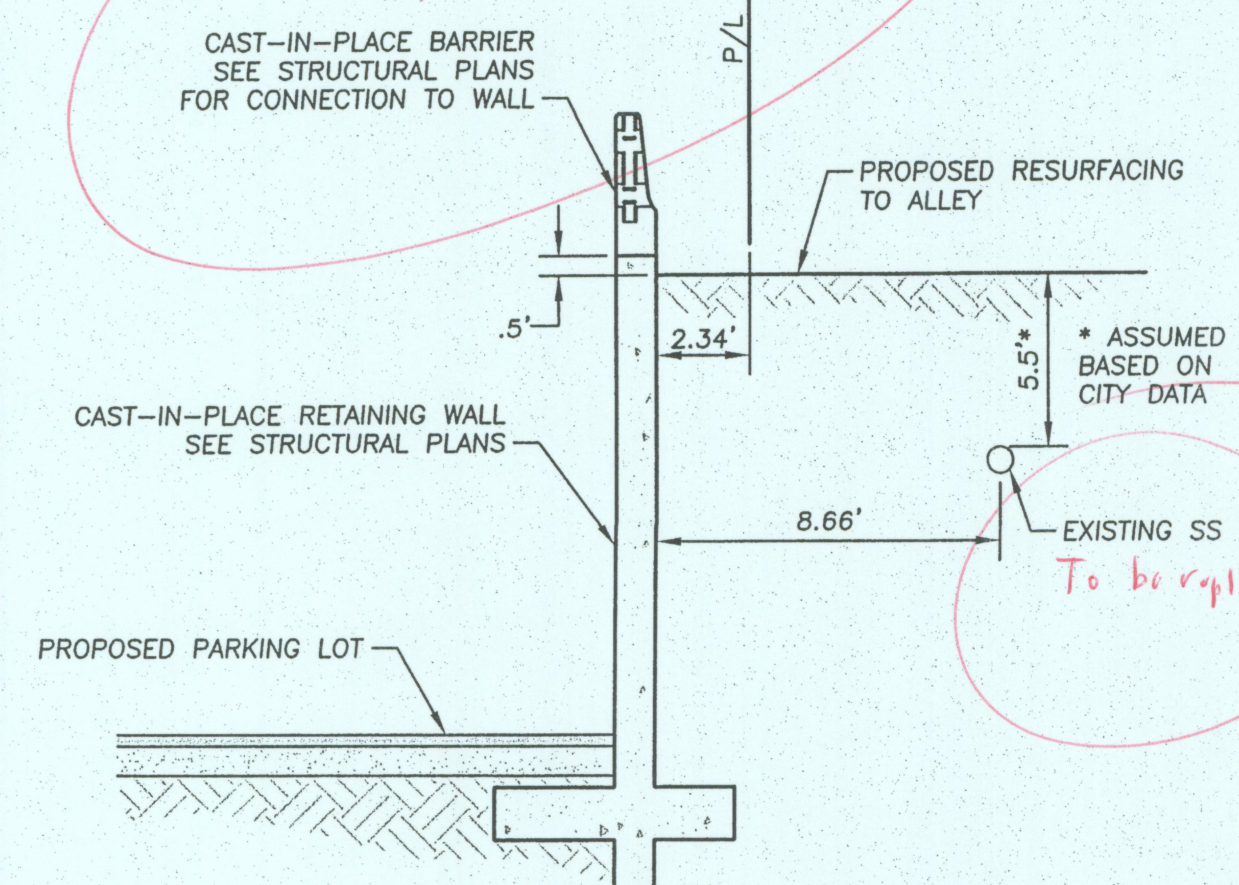


6 UTILITY TRENCH DETAIL

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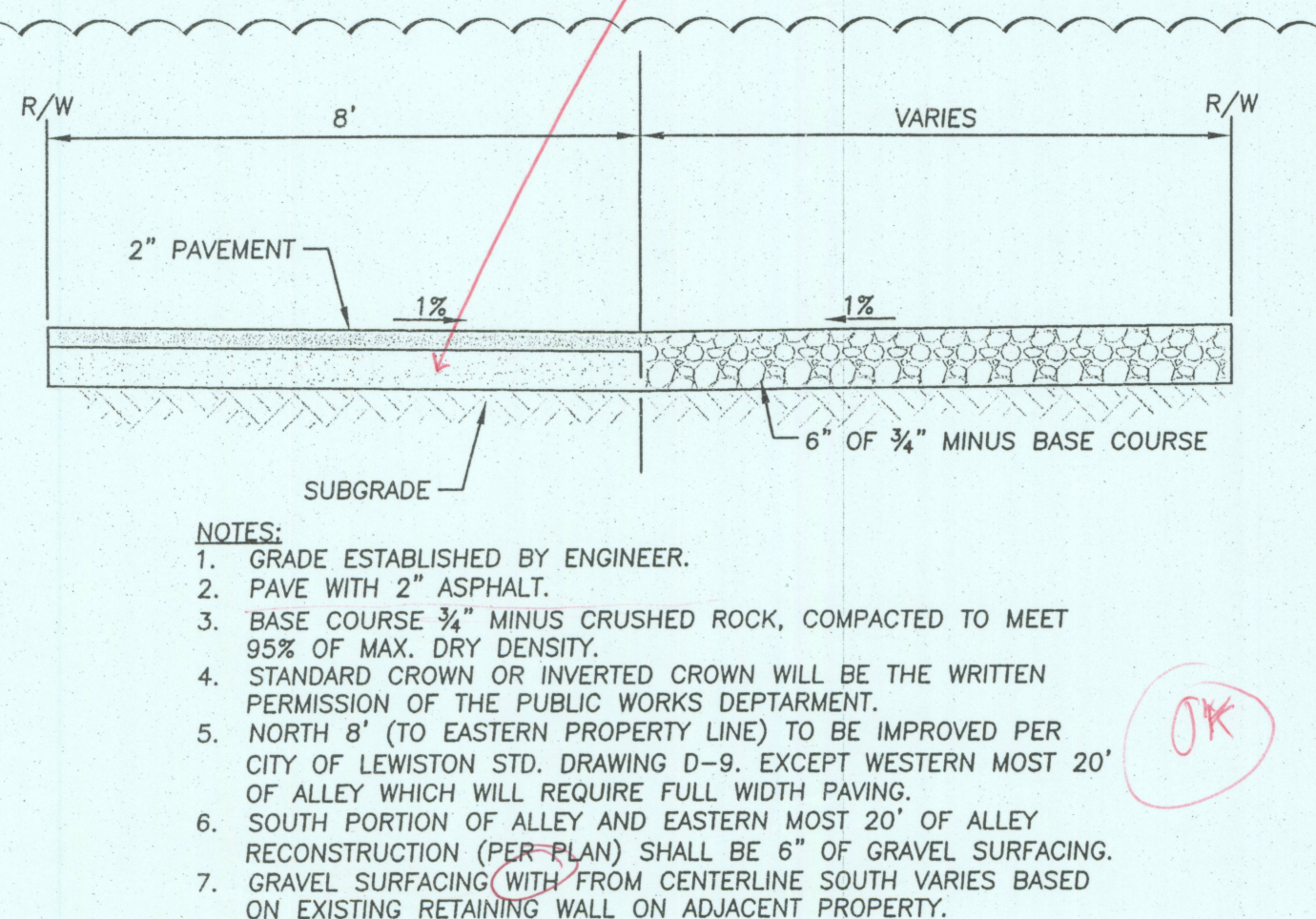
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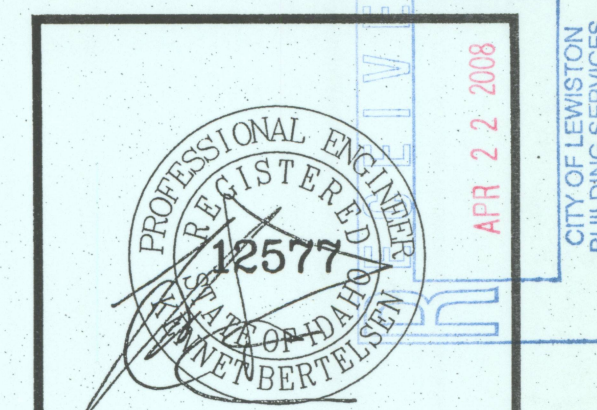
7 RETAINING WALL SECTION A-A

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8 RESIDENTIAL PAVED ALLEY

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Revision	ADDRESS CITY'S 1/30/08 COMMENTS	ADDRESS CITY'S 3/4/08 COMMENTS	ADDRESS CITY'S 3/12/08 COMMENTS
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Project:  
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Project Mgr.	GC
Drawn	ZCS ALB
Drawn	RKB
Checked	AEG KB
Date	11/05/07

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**SITE AND  
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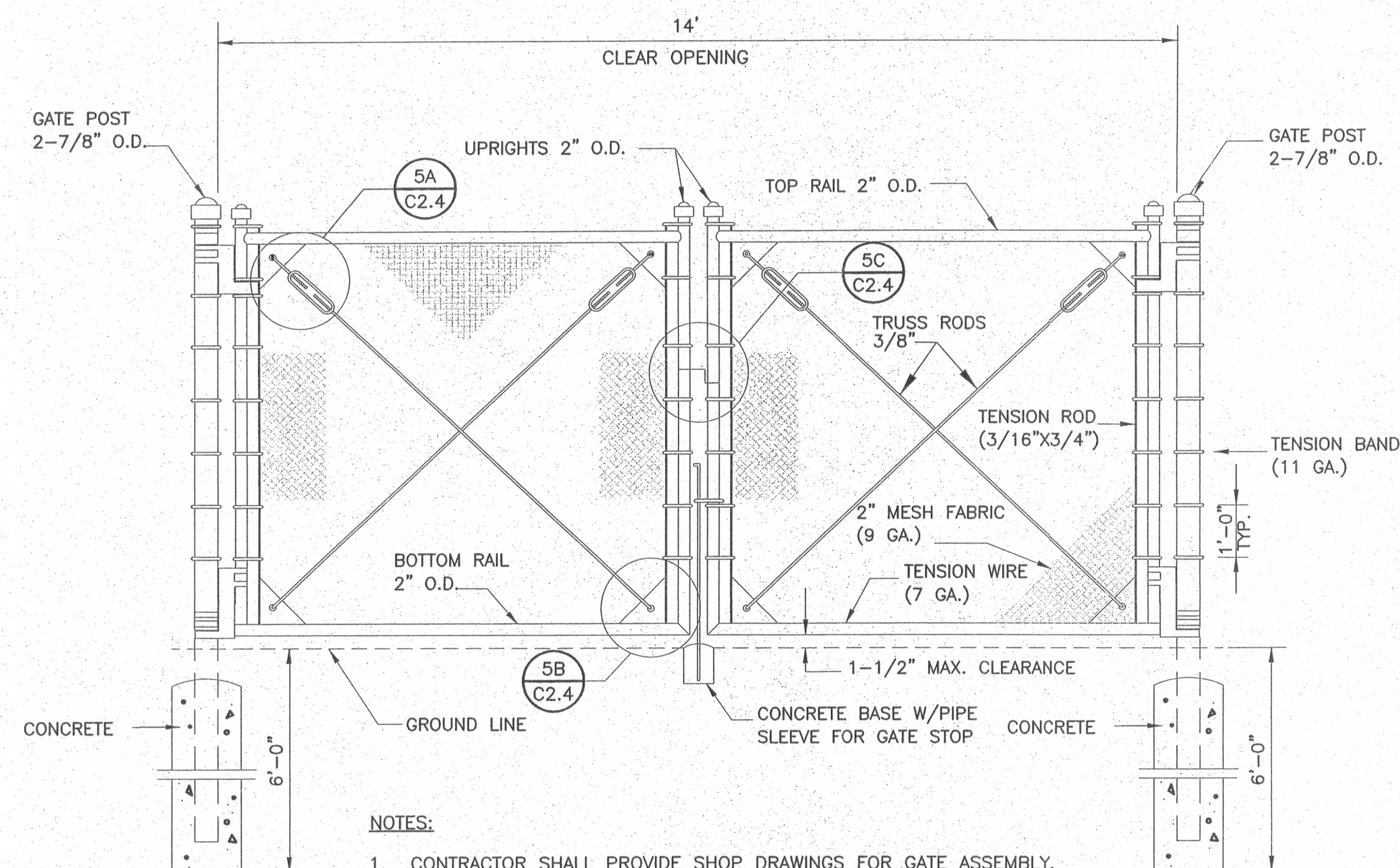
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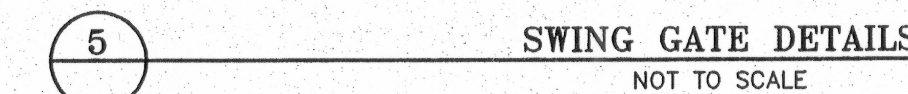
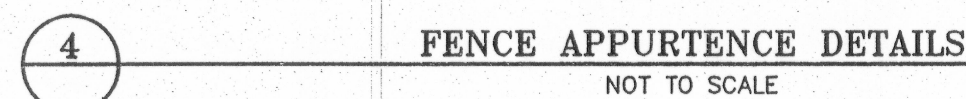
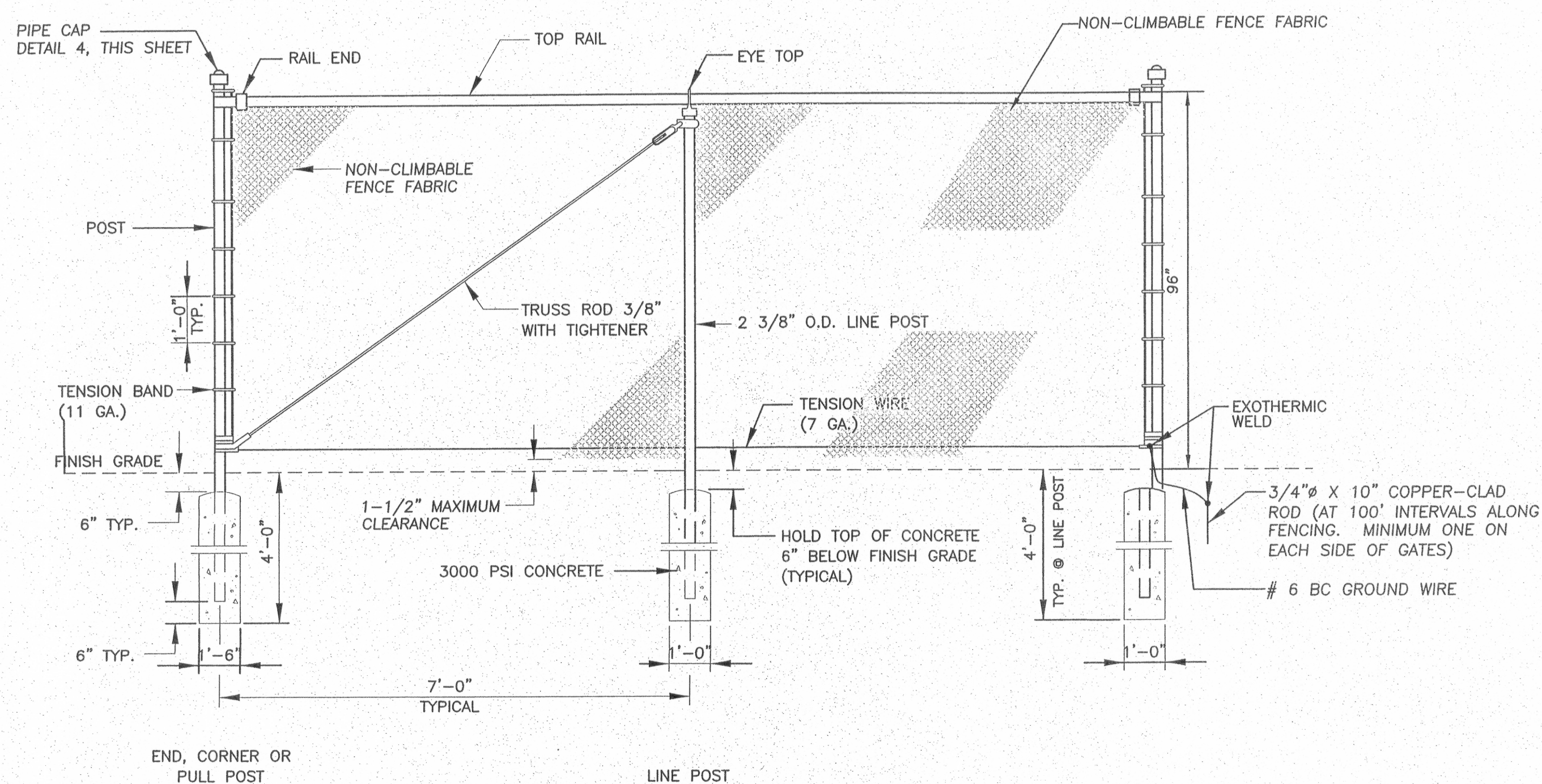
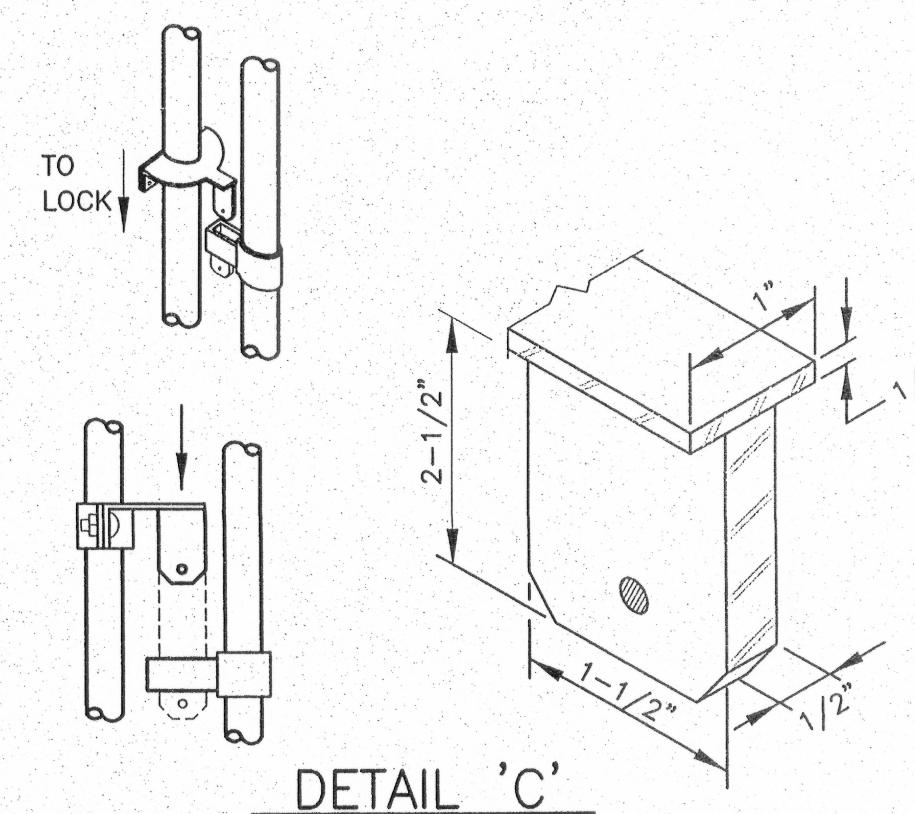
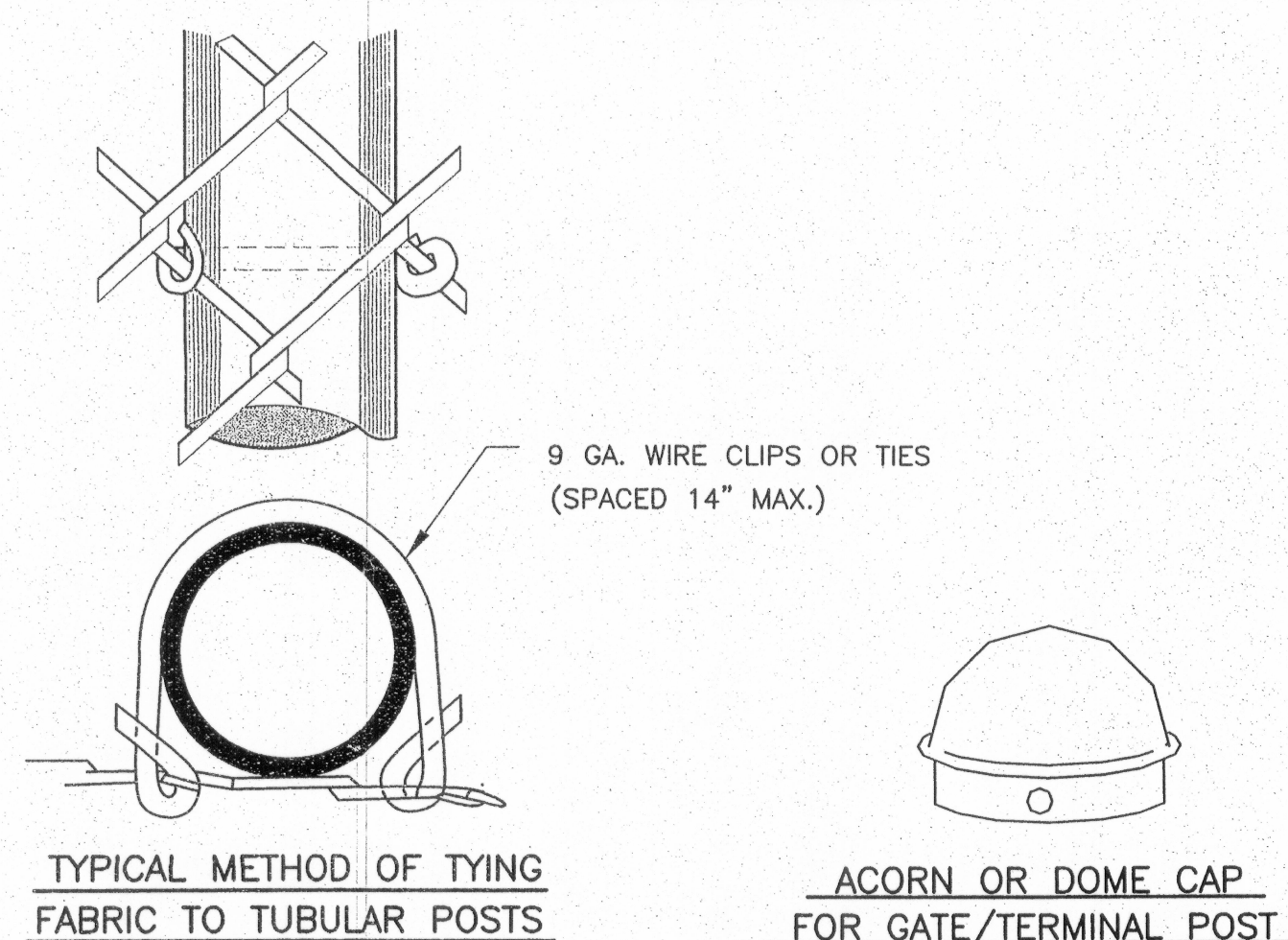
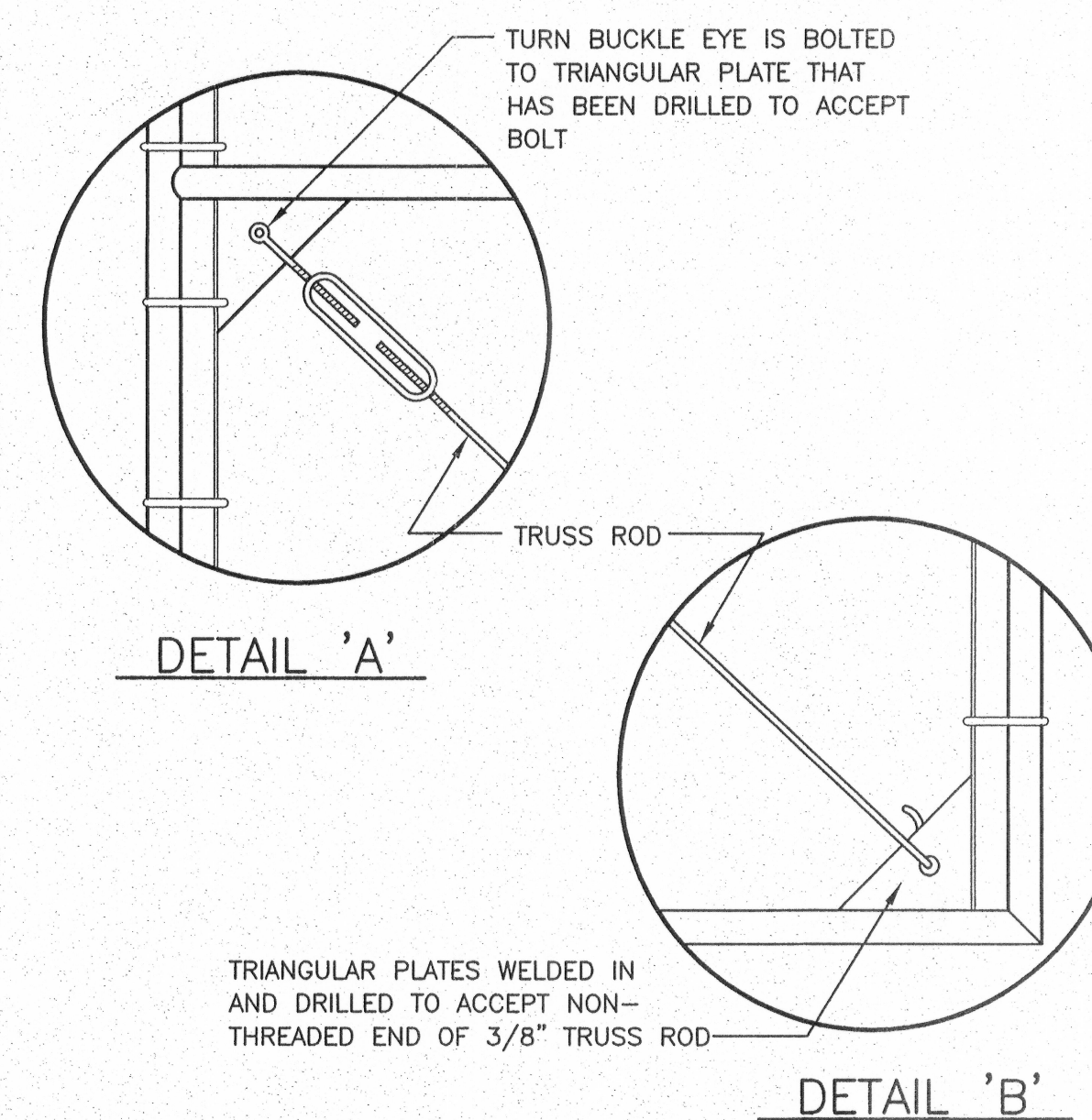
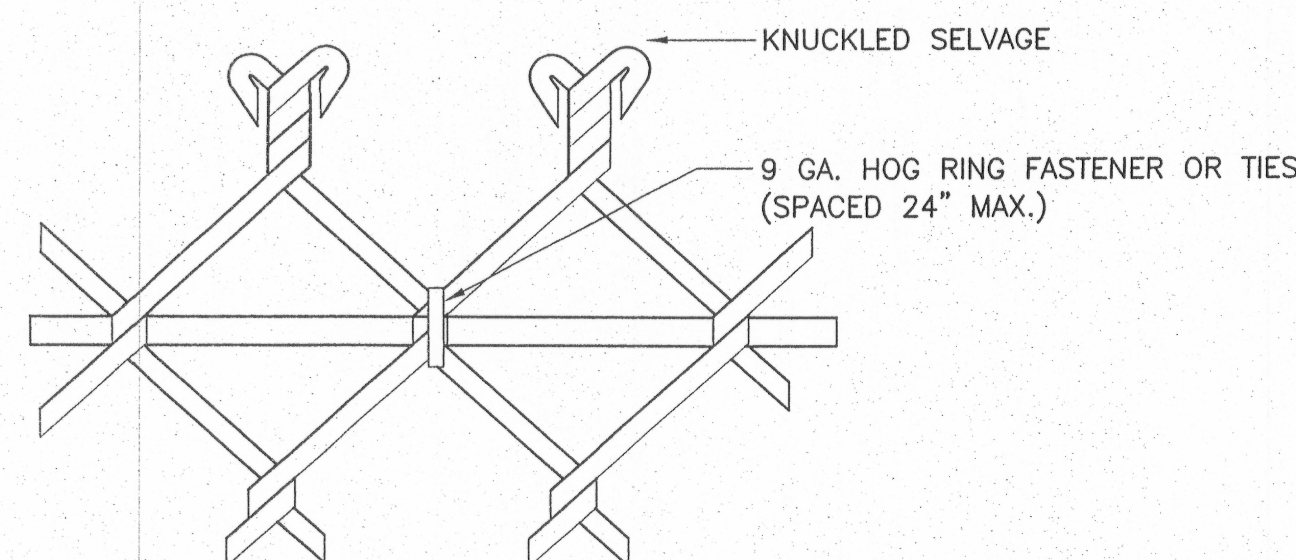
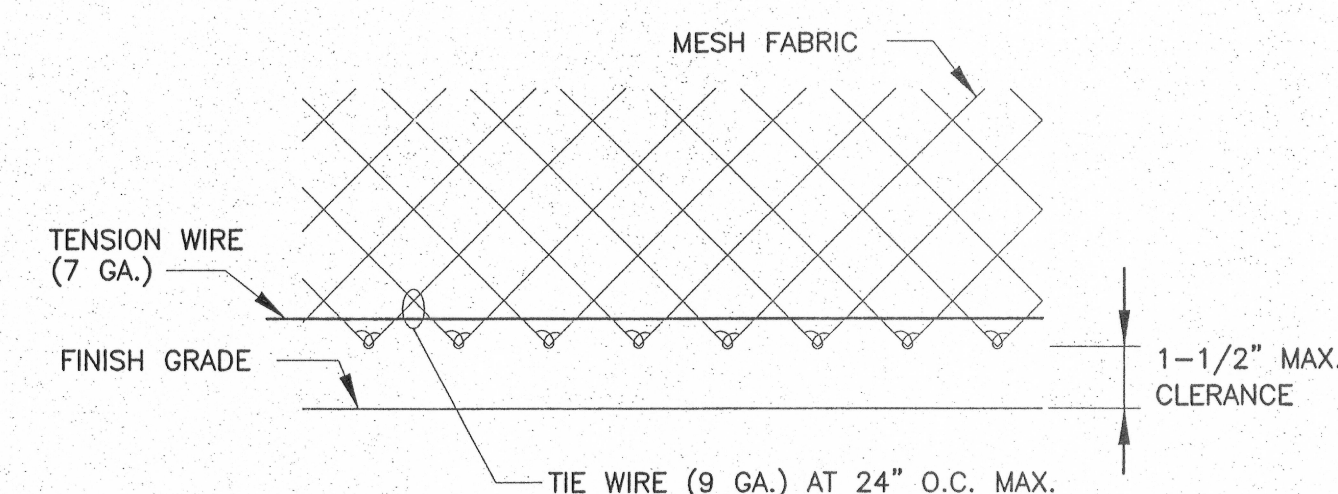
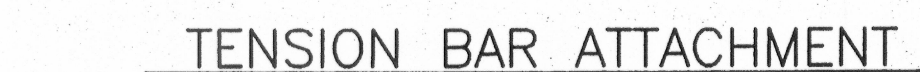
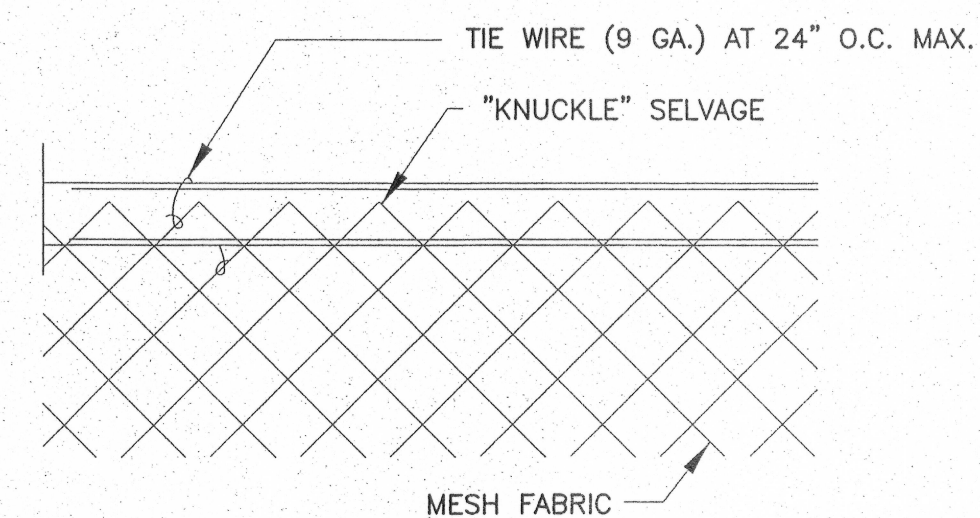
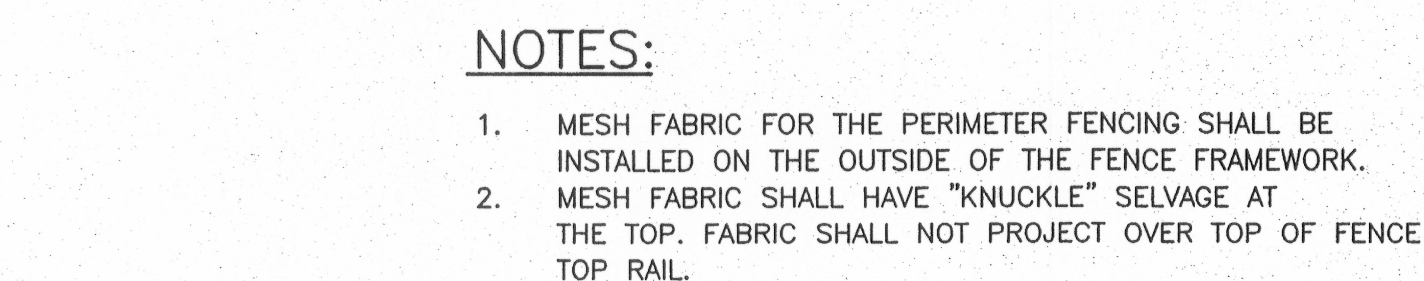
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#3





- NOTES:
1. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR GATE ASSEMBLY.
  2. CONTRACTOR SHALL PROVIDE MECHANISMS FOR LOCKING GATES IN "OPEN" POSITION.

A circular professional engineer registration stamp. The outer ring contains the text "PROFESSIONAL ENGINEER" at the top and "STATE OF TEXAS" at the bottom. The inner ring contains the word "REGISTERED". In the center, the number "12577" is printed. A diagonal line is drawn across the stamp from the bottom left to the top right. The name "J. BERTELSON" is partially visible at the bottom.

Date Stamped: 4/16/08

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Client:  
JAMES ELMER CONSTRUCTION

Project Mar.	GC
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Drawn	RKB	
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Checked	AEG	KB
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Date	04/14/08
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Sheet Contents:

## FENCE DETAILS

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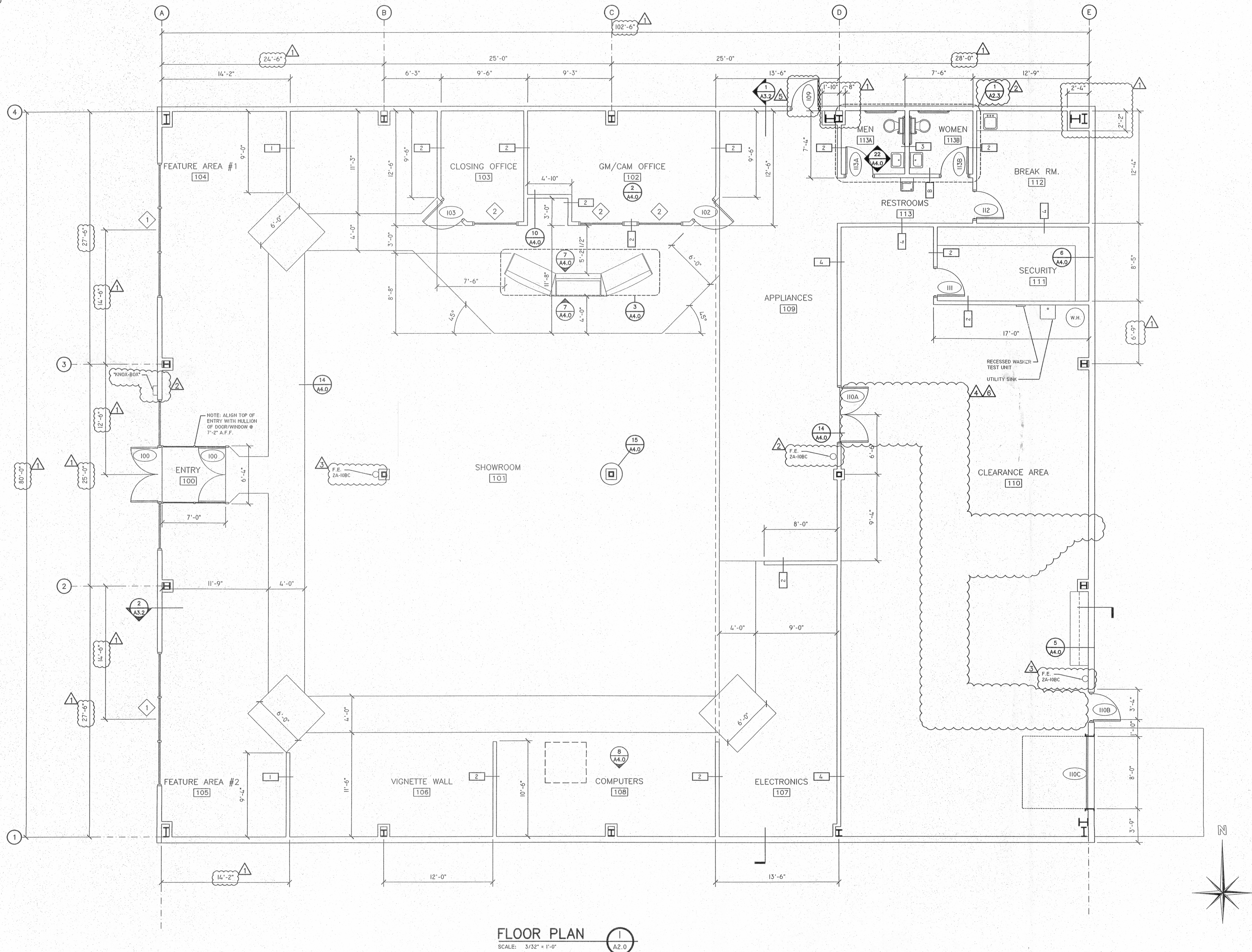
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USKH W.O. 1030300



SHEET NOTES

1. GRID LINES TO OUTSIDE FACE OF STRUCTURE.
2. UPON FINAL OCCUPANCY OR INSPECTION IF THE "CLEARANCE AREA" IS ALSO USED AS A STOCKROOM FOR INVENTORY AWAITING DISPLAY OR SALE, THIS AREA SHALL BE MADE TO COMPLY WITH IFC 101.2 FOR STORAGE ROOMS IN AN "H" OCCUPANCY AND FULL OR PARTIAL HEIGHT WALLS PROVIDING A DISCREIBLE PATH OF EGRESS 44 INCHES WIDE SHALL BE REQUIRED FROM THE ENTRANCE TO THE "CLEARANCE AREA" AT THE SHOWROOM TO THE REAR EXIT DOOR NEAR THE LOADING DOCK AT THE OWNERS EXPENSE. IT IS RECOMMENDED THAT IF THE AREA IS TO BE USED FOR ANY STORAGE OF ANY COMMODITIES, AS IS CONSISTENT WITH CORPORATE STANDARDS, THAT A SECOND EXIT IN ACCORDANCE WITH IFC SECTION 10 BE INSTALLED WITHIN THE SHOWROOMS, THIS ALLEVIATING ANY FUTURE BURDENS TO THE OWNER FOR PROVIDING A REQUIRED DISCREIBLE AND READILY AVAILABLE MEANS OF EGRESS FROM THE STRUCTURE.



FLOOR PLAN

SCALE: 3/32\" = 1'-0"

1  
A2.0

LICENSED  
ARCHITECT  
AR - 084727  
W. WRIGHT ALCONRY JR.  
STATE OF IDAHO

Date Stamped: 2-6-08

By	Revision	Date
JTP	COORDINATION WITH STRUCTURAL	12/27/07
JTP	PLAN REVIEW	2/6/08
JTP	FIRE DEPARTMENT REVIEW	3/4/08
JTP	2ND MEANS OF EGRESS	3/12/08
JTP	2ND EXTERIOR DOOR	4/3/08
JTP	REMOVAL OF EGRESS STRIPPING	4/16/08

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Project:  
**AARON SALES  
& LEASING**

Client  
ASPEN WAY DEVELOPMENT, LLC  
JAMES ELMER CONSTRUCTION

Project Mgr.	GTC
Drawn	JTP
Drawn	LEH
Checked	
Date	11/5/07

Sheet Contents:

FLOOR PLAN

Sheet No.:

A2.0

USKH W.O. 1030300

ENGINEERING DEPT.  
COPY

ENGINEERING DEPT.  
COPY

B08-000007#3

1107 21<sup>ST</sup> ST.

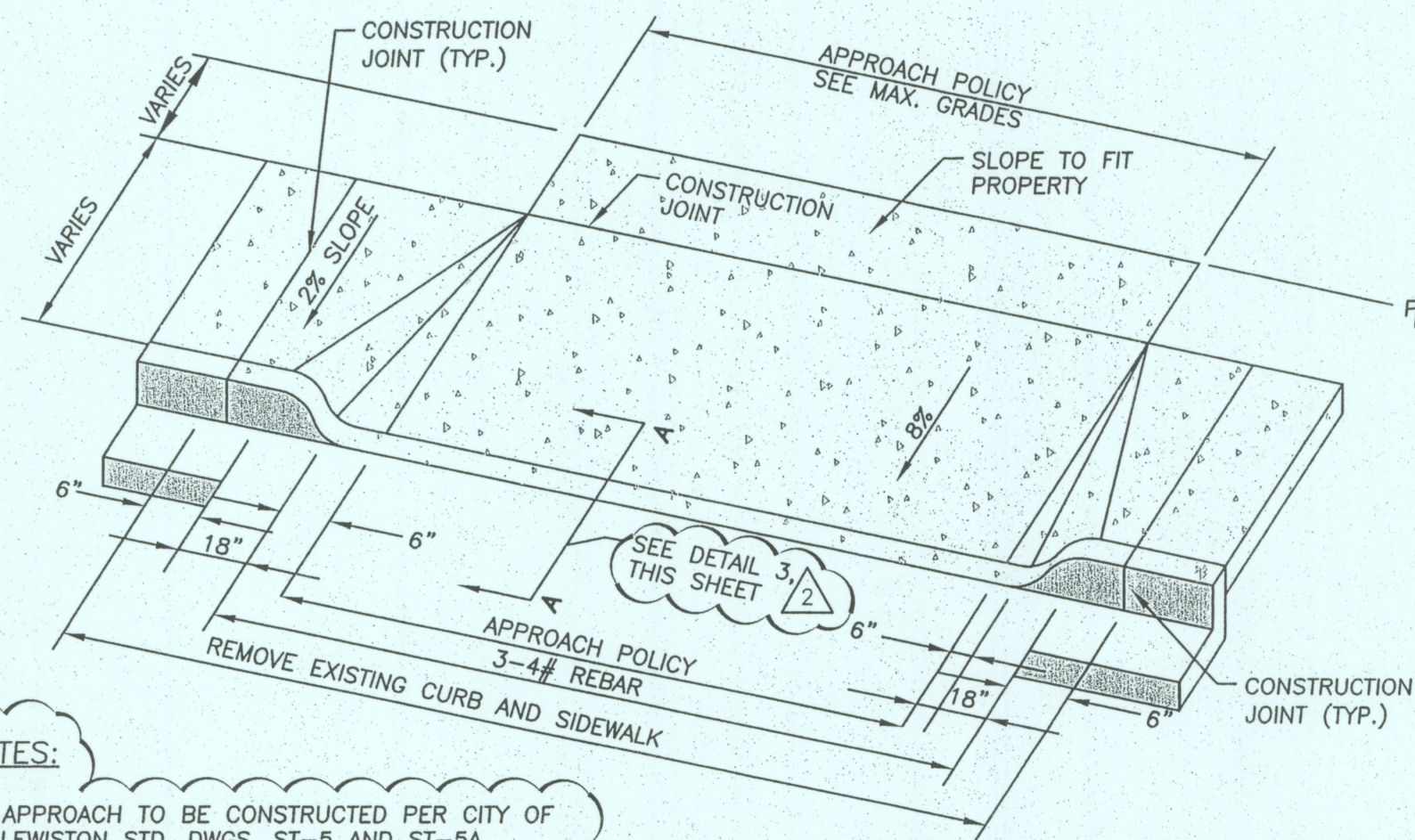
New Comm. Retail Bldg 8,058



[illegible]

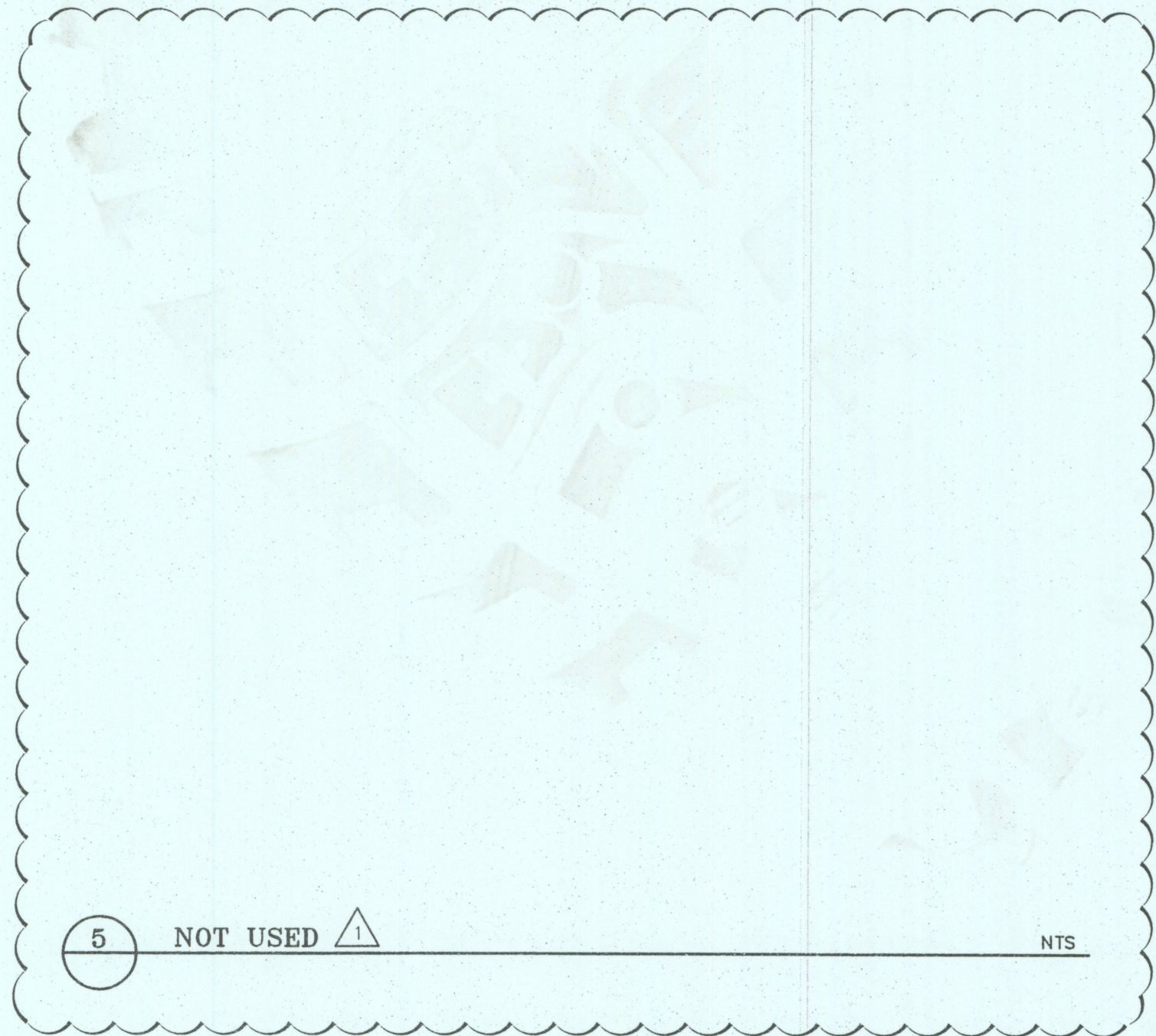
IN. OR " - INCHES  
MAX - MAXIMUM  
MH - MANHOLE  
MIN. - MINIMUM  
N - NORTH  
NIC - NOT IN CONTRACT  
NO. or # - NUMBER  
PROP. - PROPOSED  
RT - RIGHT  
S - SOUTH  
SF - SQUARE FEET  
STA. - STATION  
TBC - TOP BACK OF CURB  
TOS - TOP OF SIDEWALK  
TOW - TOP OF WALL  
TRW - TOP OF RETAINING WALL  
TYP. - TYPICAL  
VG - VALLEY GUTTER  
W - WEST



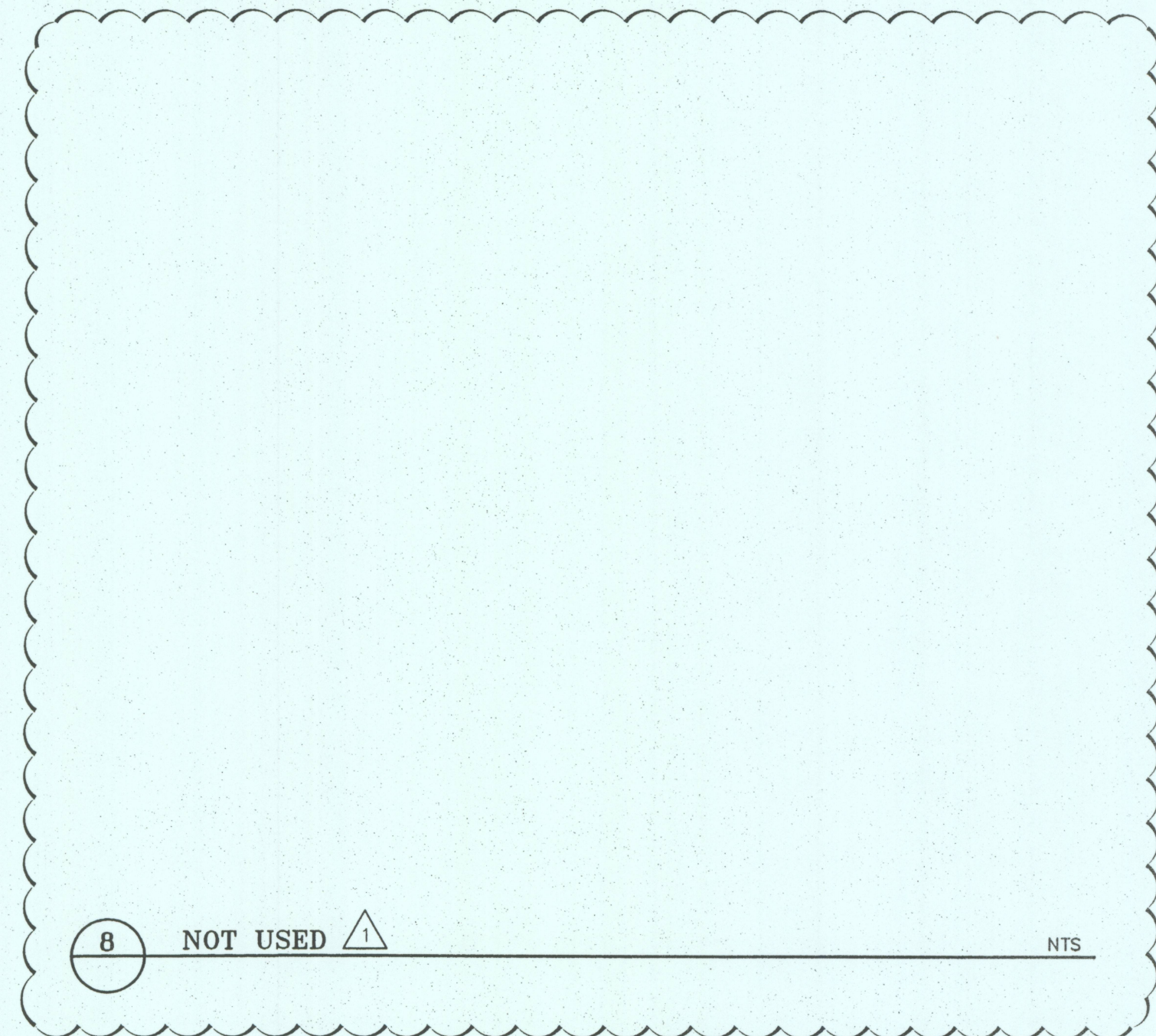


- NOTES:**
1. APPROACH TO BE CONSTRUCTED PER CITY OF LEWISTON STD. DWGS. ST-5 AND ST-5A.
  2. SIDEWALK WIDTH TO MATCH EXISTING.

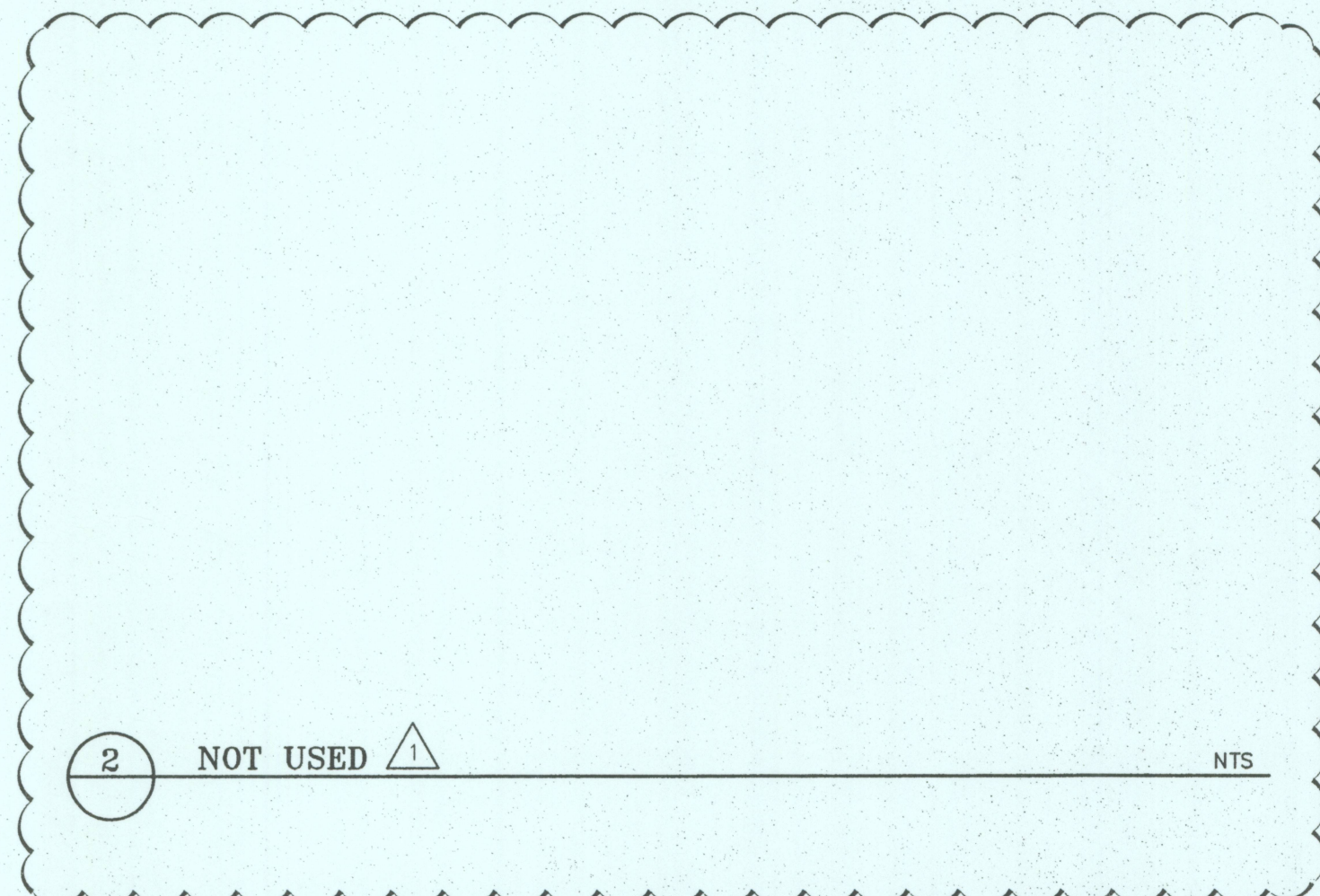
**1 STANDARD APPROACH** NTS



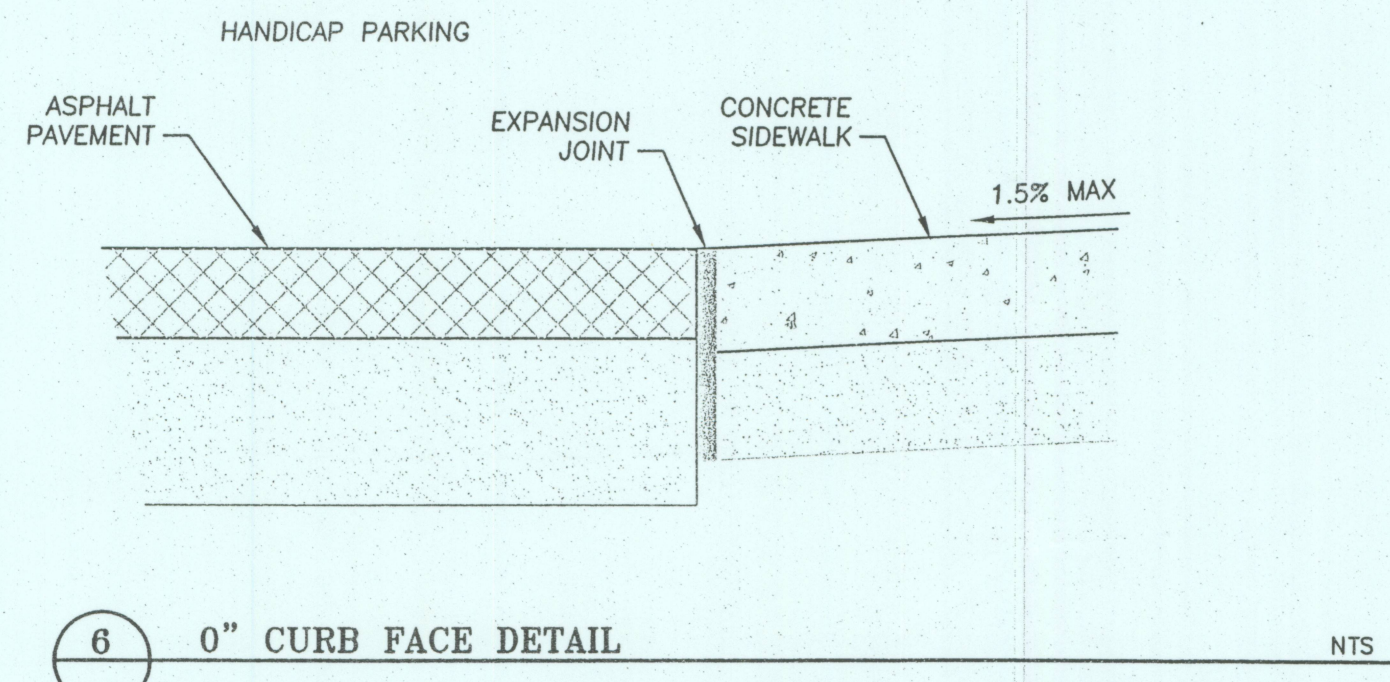
**5 NOT USED** NTS



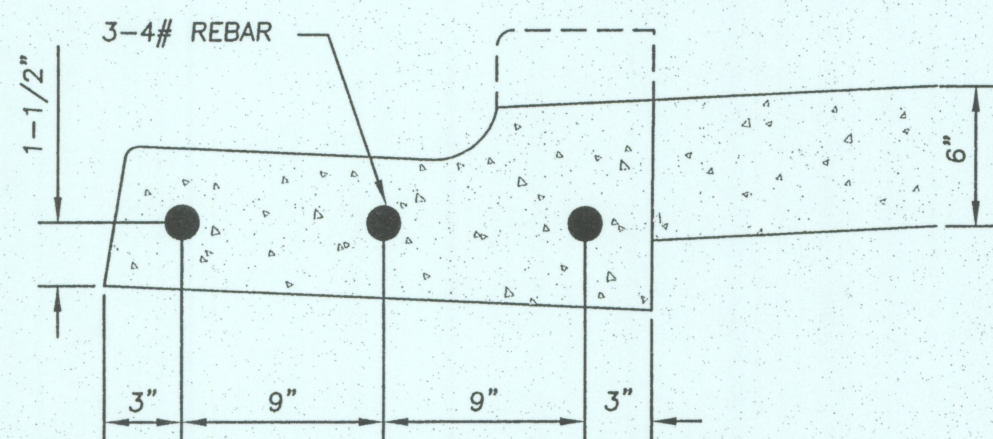
**8 NOT USED** NTS



**2 NOT USED** NTS

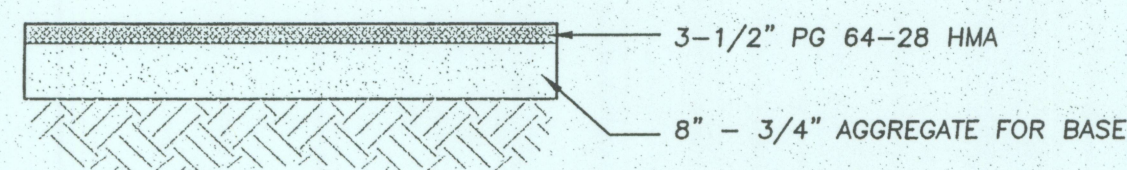


**6 0" CURB FACE DETAIL** NTS

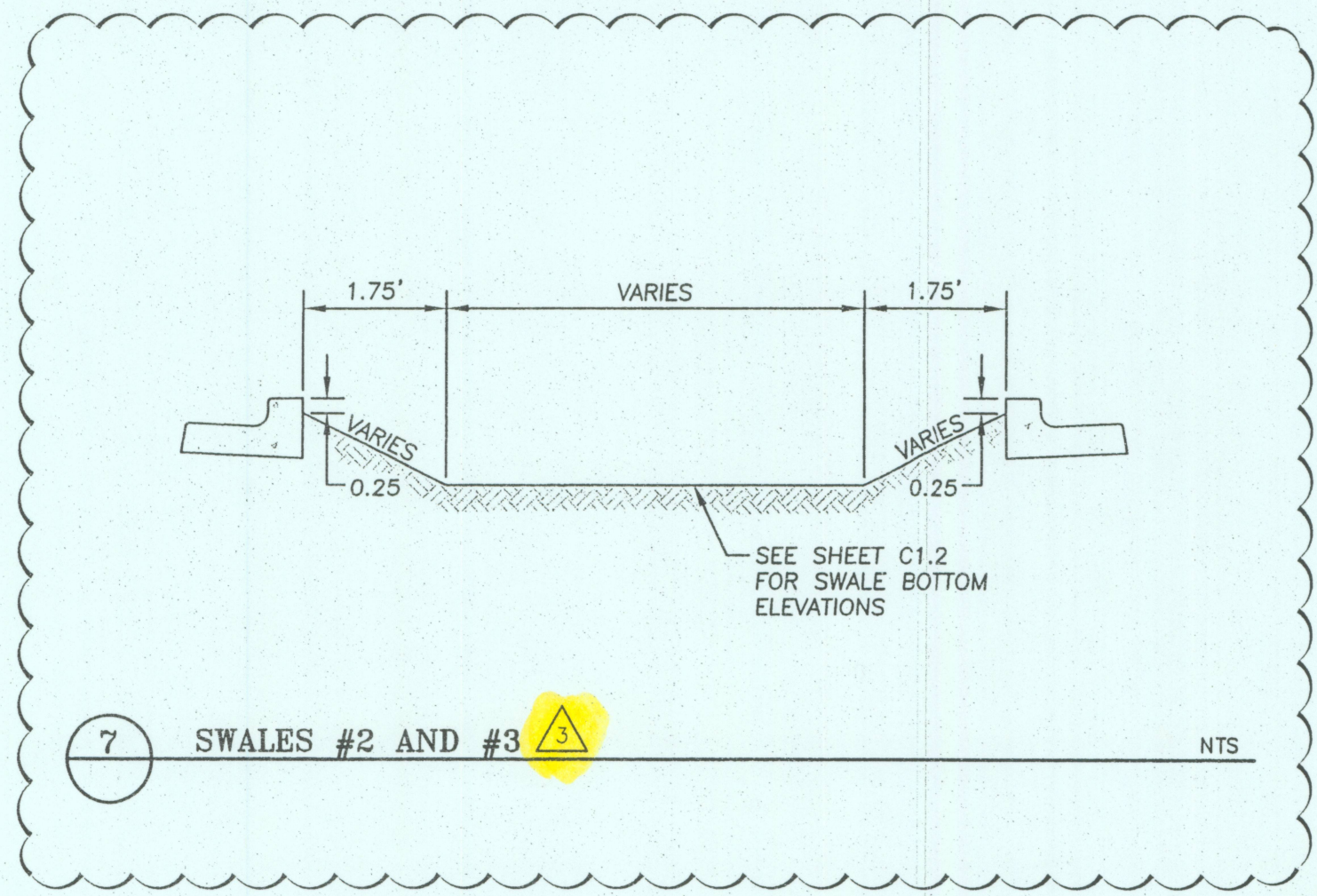


REBAR ONLY REQUIRED IN APPROACH, CITY OF LEWISTON STD. DWG. ST-5, OPTION 2

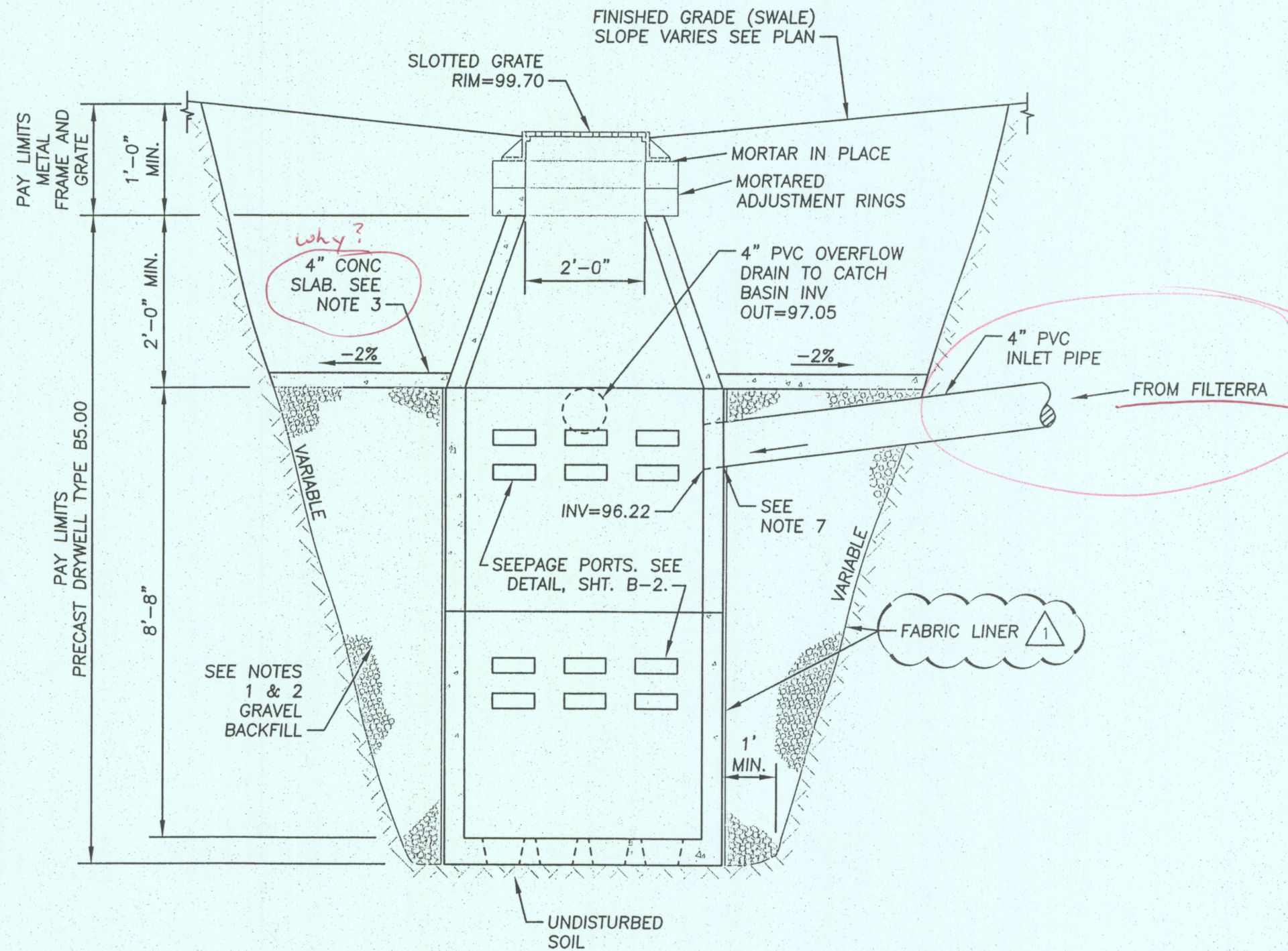
**3 CURB & GUTTER REBAR DETAIL** NTS



**4 PAVEMENT SECTION DETAIL** NTS



**7 SWALES #2 AND #3** NTS



**GENERAL NOTES**

1. GRAVEL BACKFILL QUANTITY FOR DRYWELLS :  
TYPE "A" - 30 CUBIC YARDS MINIMUM / 42 TONS.  
TYPE "B" - 40 CUBIC YARDS MINIMUM / 56 TONS  
OR AS SPECIFIED ON ROAD PLANS.
2. SPECIAL BACKFILL MATERIAL FOR DRYWELLS SHALL CONSIST OF WASHED GRAVEL GRADED FROM 1" TO 3" WITH A MAXIMUM OF 5% PASSING THE U.S. No. 200 SCREEN, AS MEASURED BY WEIGHT. A MAXIMUM OF 10% OF THE AGGREGATE, AS MEASURED BY WEIGHT, MAY BE CRUSHED OR FRACTURED ROCK. THE REMAINING 90% SHALL BE NATURALLY OCCURRING UNFRACTURED MATERIAL.
3. CONCRETE SLAB SHALL BE CLASS 3000 CONCRETE.
4. SEE STANDARD PLANS SHEETS B-2 AND B-3 FOR PRECAST CONCRETE DETAILS.
5. ADJUSTMENT BLOCKS SHALL BE CEMENT CONCRETE.
6. PRECAST RISER MAY BE USED IN COMBINATION WITH OR IN LIEU OF ADJUSTING BLOCKS.
7. WHEN PVC PIPE IS USED A PVC ADAPTER SHALL BE INSTALLED.
8. PIPES SHALL BE GROUTED INTO DRYWELLS.

**9 DRYWELL DETAIL** NTS

APR 22 2008  
CITY OF LEWISTON  
BUILDING SERVICES

Date Stamped: 4/16/08			
By	ALB	ALB	
Revision	ADDRESS CITY'S 1/30/08 COMMENTS	ADDRESS CITY'S 3/12/08 COMMENTS	
Date	2/6/08	3/24/08	

**USKH**  
Engineering • Architectural • Planning  
Land Surveying • Environmental

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Project:  
**AARON'S SALES & LEASING**

Client: JAMES ELMER CONSTRUCTION	
Project Mgr.	GC
Drawn	ZCS ALB
Drawn	RKB
Checked	AEG KB
Date	11/05/07

Sheet Contents:  
**SITE AND DRAINAGE DETAILS**

Sheet No.:  
**C2.1**

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